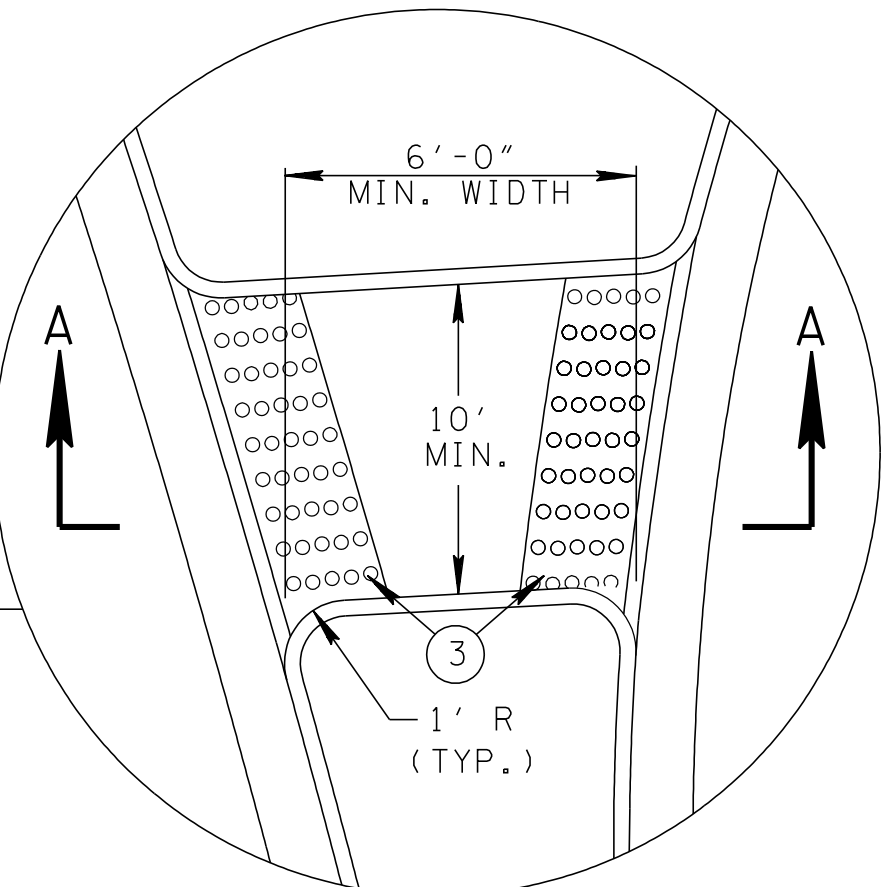
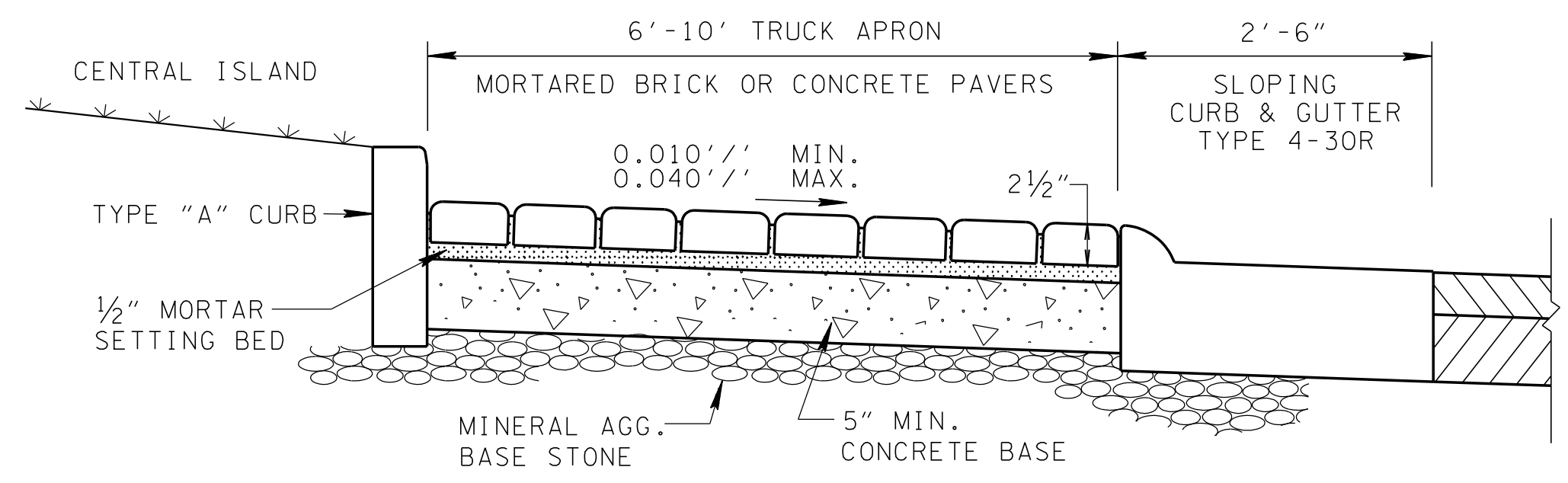


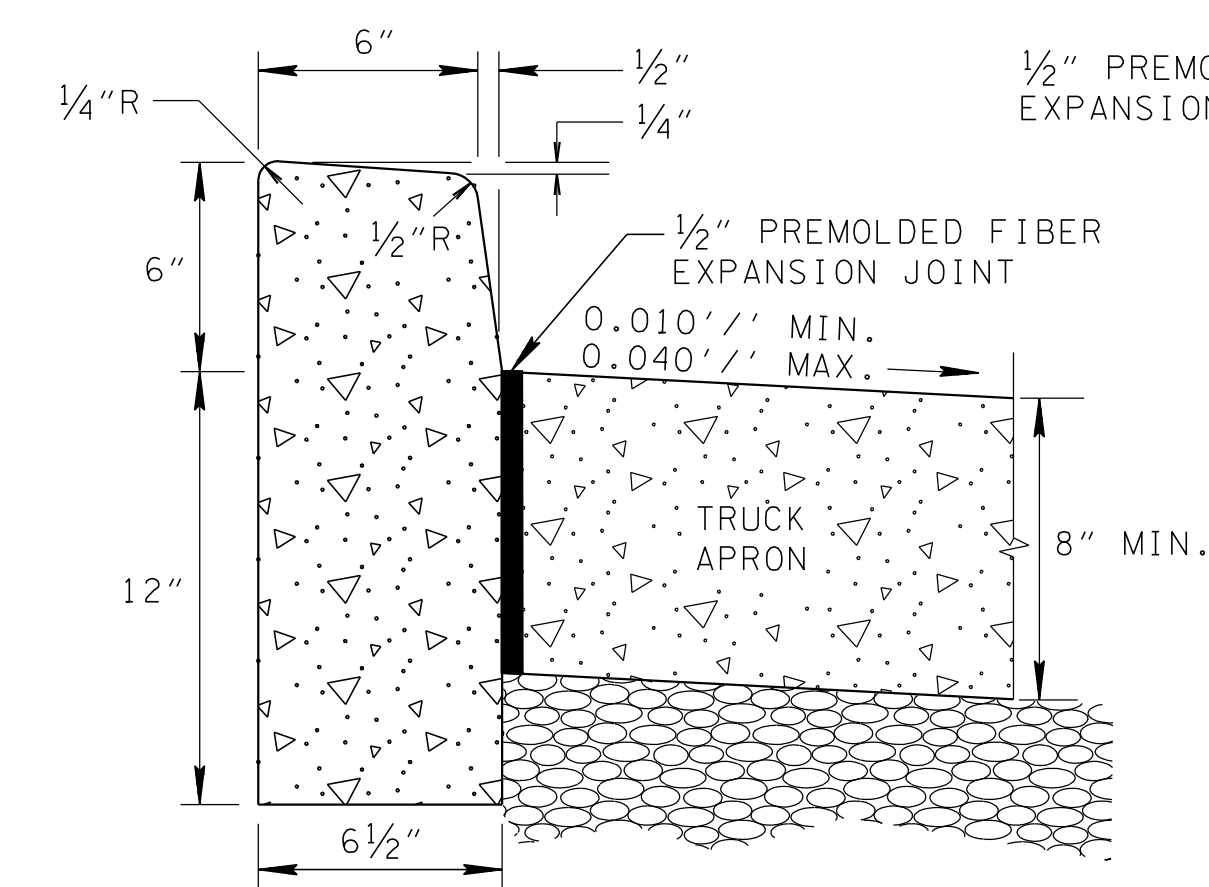
TYPICAL SPLITTER ISLAND



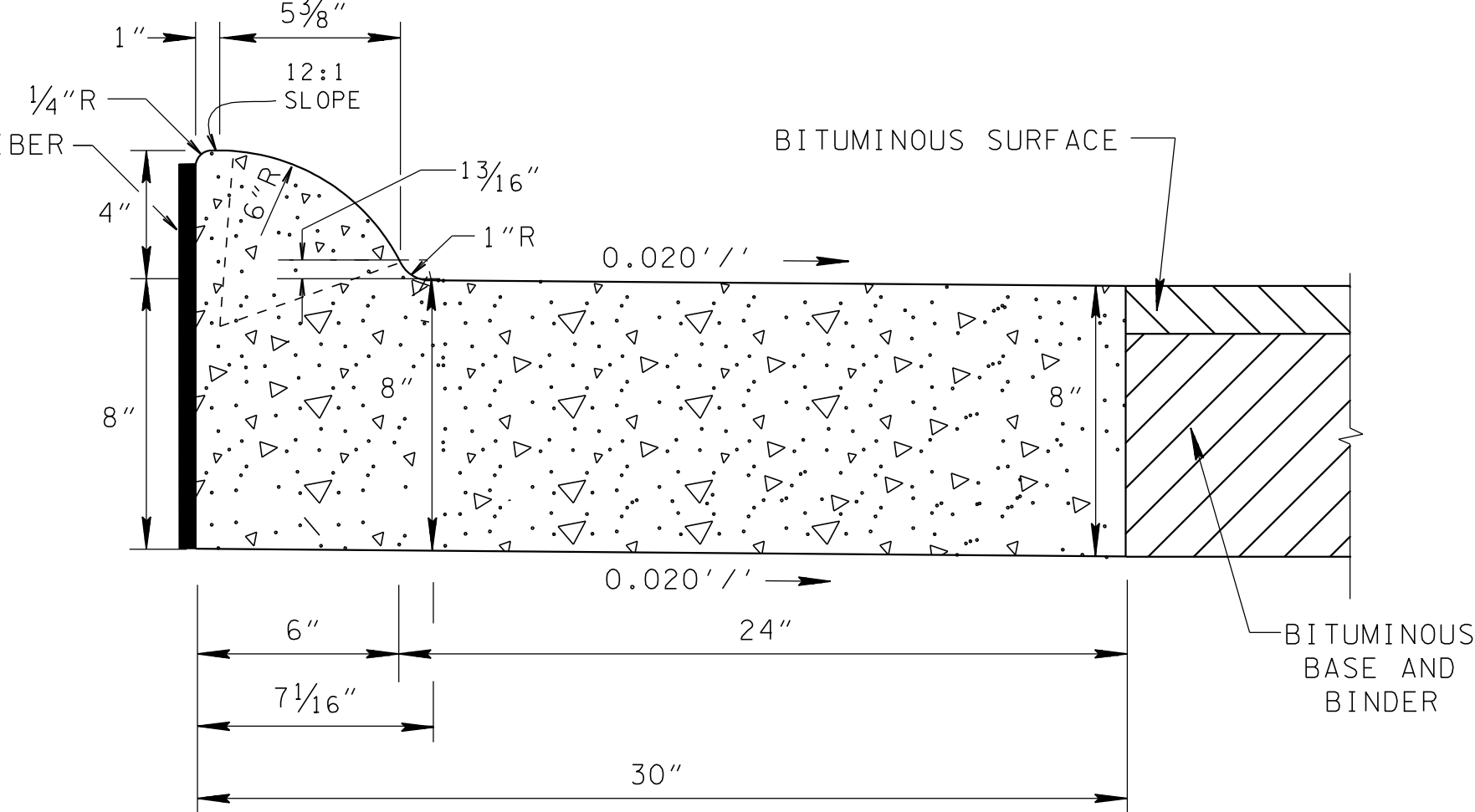
PEDESTRIAN REFUGE AREA AT SPLITTER ISLAND



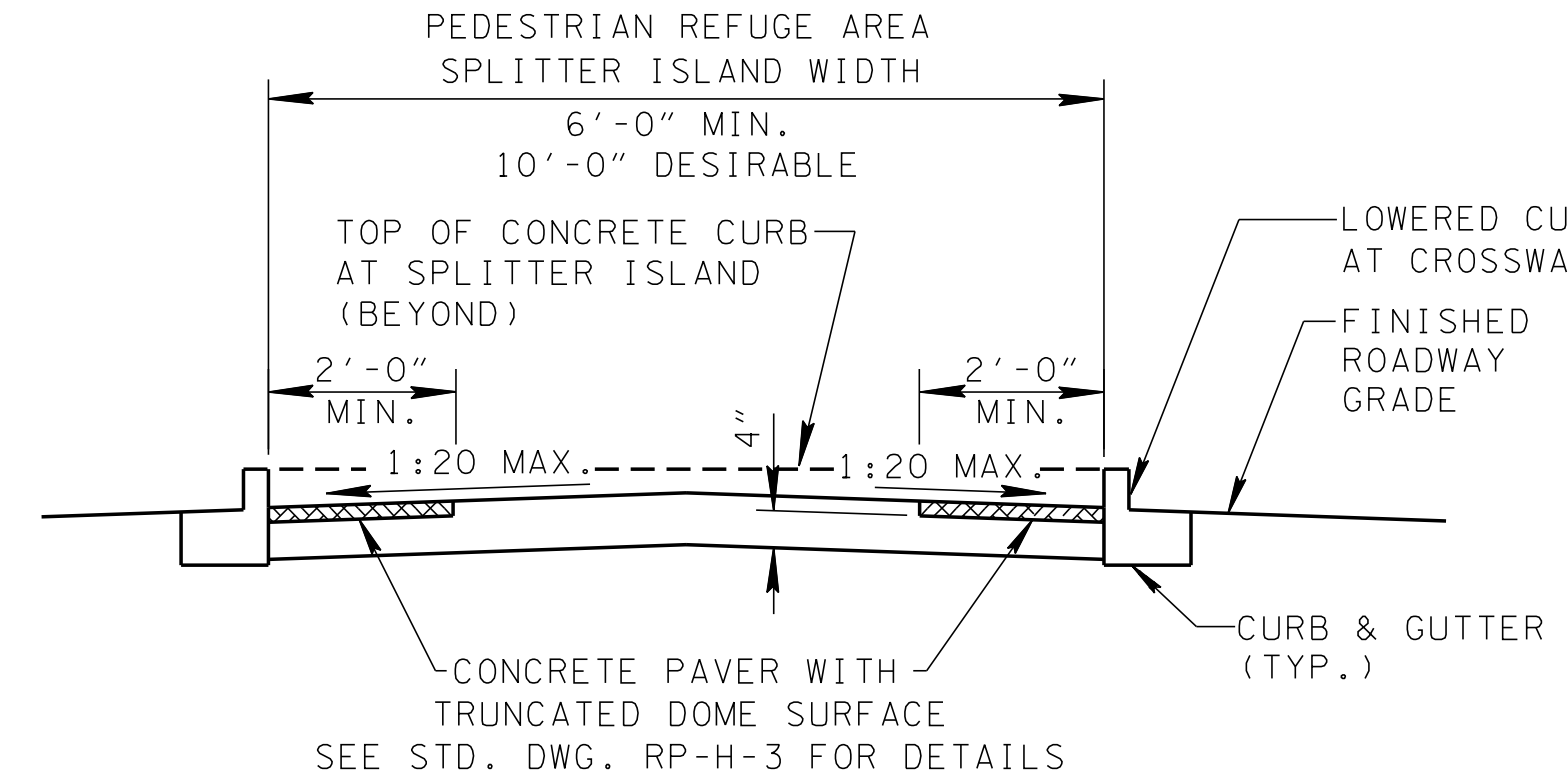
TRUCK APRON WITH CONCRETE OR BRICK PAVERS  
(BRICK PAVERS SHOWN)



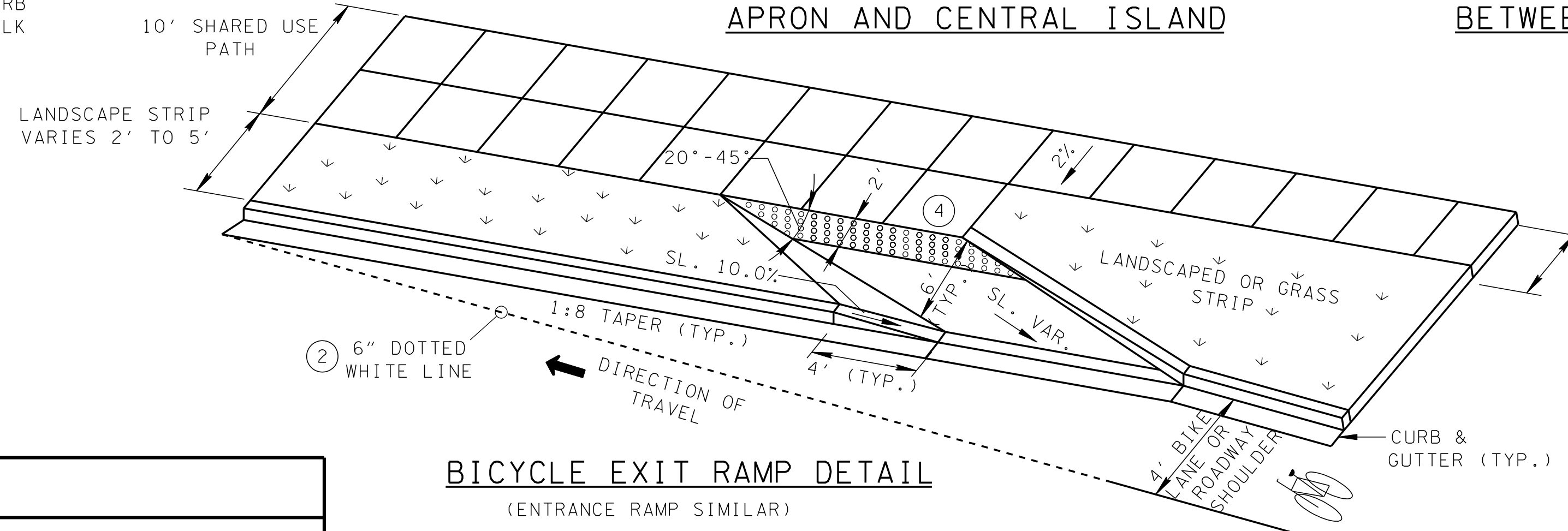
TYPE "A" CURB BETWEEN TRUCK APRON AND CENTRAL ISLAND



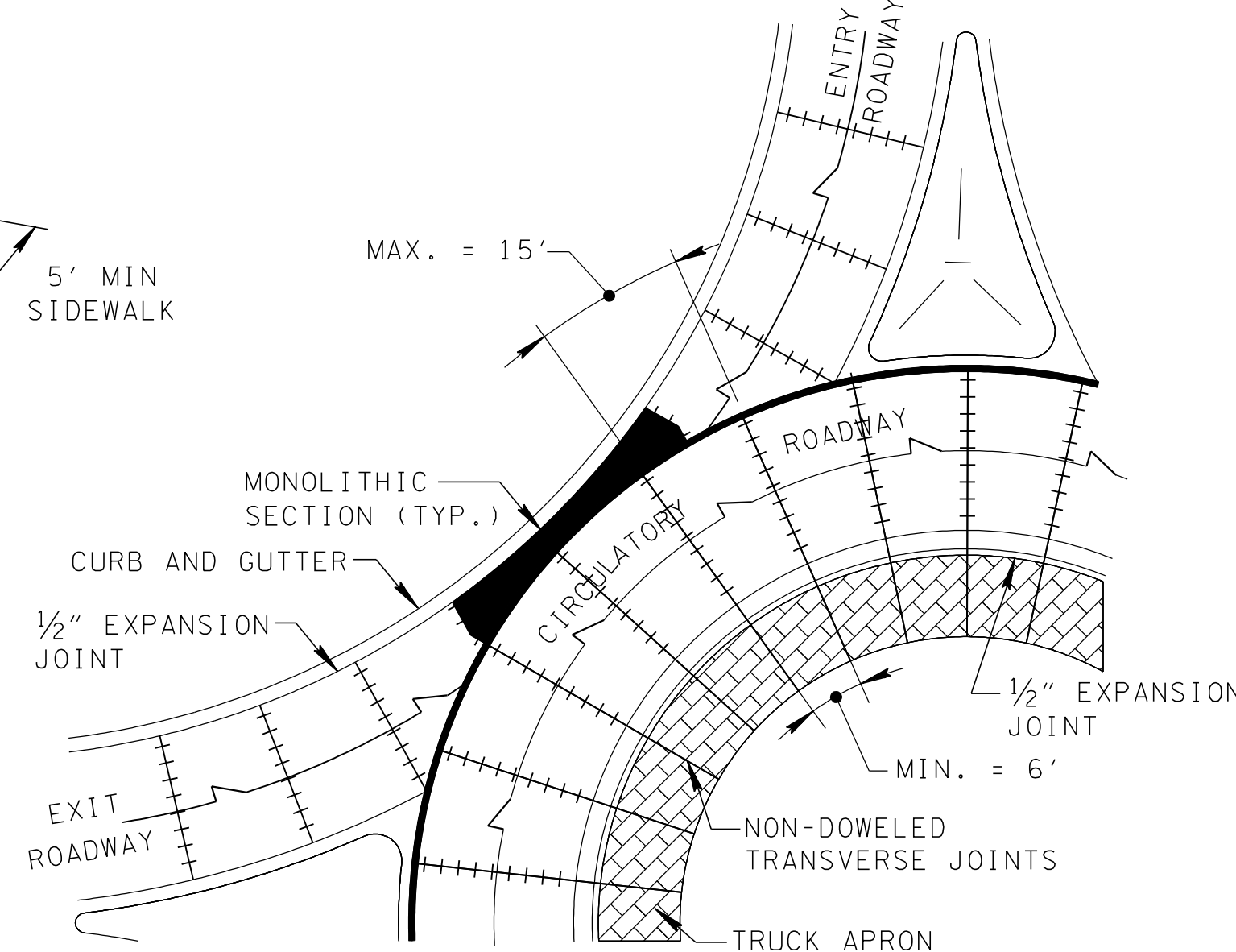
4" SLOPING CONCRETE COMBINED CURB AND GUTTER BETWEEN CIRCULATORY ROADWAY AND TRUCK APRON  
TYPE 4-30R



SPLITTER ISLAND CROSSING SECTION A-A



BICYCLE EXIT RAMP DETAIL  
(ENTRANCE RAMP SIMILAR)



TYPICAL JOINTING DETAIL FOR CONCRETE PAVEMENT

**GENERAL NOTES**

(A) 4" SLOPING CONCRETE COMBINED CURB AND GUTTER SHOULD BE USED BETWEEN CIRCULATORY ROADWAY AND TRUCK APRON UNLESS OTHERWISE NOTED. TYPE "A" CURB SHOULD BE USED BETWEEN THE TRUCK APRON AND THE CENTRAL ISLAND.

(B) THE CROSS SLOPE OF THE LANDING AREA SHALL NOT EXCEED 2% IN THE SIDEWALK AREA.

(C) SPLITTER ISLAND SIZE AND SHAPE WILL BE DETERMINED BY THE ROADWAY DEFLECTION.

(D) THE FINISH ON THE TRUCK APRON SHOULD CREATE A CONTRAST IN COLOR OR TEXTURE BETWEEN THE CIRCULATORY ROADWAY AND THE APRON. THIS CAN BE ACCOMPLISHED WITH THE USE OF CONCRETE, STAMPED CONCRETE, COLORED CONCRETE, CONCRETE PAVERS, OR BRICK PAVERS. WHEN PAVERS ARE USED, A BASKETWEAVE OR HERRINGBONE PATTERN SHOULD BE APPLIED.

(E) FOR PAVEMENT MARKINGS AND SIGNAGE AT BICYCLE RAMP AND SHARED USE PATH, SEE STANDARD DRAWING T-M-10.

(F) UNLESS OTHERWISE NOTED ON PLANS, THE CIRCULATORY ROADWAY SHOULD BE CONSTRUCTED OF ASPHALT. THE USE OF CONCRETE PAVEMENT SHALL BE ON A CASE-BY-CASE BASIS.

(G) CONCRETE QUANTITY FOR TYPE 4-30R CURB & GUTTER SHALL BE COMPUTED USING 0.06731 CUBIC YARDS PER LINEAR FOOT. PAYMENT WILL BE AS FOLLOWS:

ITEM NO. 702-01 CONCRETE CURB PER CUBIC YARD  
 ITEM NO. 702-02 CONCRETE GUTTER PER CUBIC YARD  
 ITEM NO. 702-03 CONCRETE COMBINED CURB AND GUTTER PER CUBIC YARD

**DESIGN NOTES**

(1) SPLITTER ISLAND SHOULD BE A RAISED MEDIAN WITH CONCRETE HARDSCAPING (PREFERRED). SPLITTER ISLAND SHOULD EXTEND A MINIMUM OF 50' FROM THE YIELD LINE. SEE STANDARD DRAWING RP-H-6 FOR ADDITIONAL DETAILS OF MEDIAN CROSSINGS.

(2) 6" X 2' DOTTED WHITE LINE ALONG ENTIRE LENGTH OF TAPER AT BICYCLE RAMP.

(3) DETECTABLE WARNING SURFACE SHALL BE INSTALLED AT BOTH SIDES OF THE SPLITTER ISLAND PEDESTRIAN REFUGE AREA. SEE STD. DWG. RP-H-3 FOR DETAILS.

(4) DETECTABLE WARNING SURFACE SHALL BE INSTALLED AT THE TOP OF ALL BICYCLE EXIT AND ENTRANCE RAMPS, SEE STD. DWG. RP-H-3 FOR DETAILS.

(5) WHEN CIRCULATORY ROADWAY IS CONCRETE, THE TRANSVERSE CONTRACTION JOINTS SHOULD LINE UP WITH CONTRACTION JOINTS IN THE TRUCK APRON. THE JOINTS IN THE TRUCK APRON SHOULD NOT BE DOWELED. THE COMBINED CURB AND GUTTER SHOULD BE TIED TO THE ROADWAY CONCRETE.

(6) FOR ADDITIONAL DETAILS OF TYPE "A" CURB, SEE STANDARD DRAWING RP-NMC-10.

