

NEW ASPHALT OVERLAY

A DETAIL SHOWING CONCRETE PARAPET DETAILS NEAR END OF BRIDGE IN ENDWALL AREA)

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REINFORCING STEEL

4" (-)

SHOWING CONCRETE PARAPET DETAILS FOR BRIDGES WITH SIDE BY SIDE PRESTRESSED BOX BEAMS OR CHANNELS)

4" (-)

NEW ASPHALT OVERLAY

TOP OF EXISTING PRESTRESSED BEAM

MOTE, WHEN PLACING NEW CONCRETE PARAPET RAIL ACROSS BRIDGES WITH EXISTING SIDEWALKS, THE EXISTING BRIDGERAIL SHALL REMAIN IN PLACE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPORTING THE EXISTING SIDEWALK CURB CANTILEYER WHEN POURING THE NEW JERSEY SHAPE CONCRETE PARAPET. COST OF SIDEWALK CURB SUPPORTS SHALL BE INCLUDED IN ITEM NO. 620-10.01.

B DENOTES: NEW HOLES FOR GROUTED BARS SHALL BE DRILLED WITH A HIGH SPEED DRILL. THE DRILL BIT SHALL BE CAPABLE OF DRILLING THROUGH EXISTING REINFORCING BARS AND CONCRETE, THE DIAMETER OF THE HOLES SHALL BE 1/2 (+) LARGER THAN THE DIAMETER OF THE REINFORCING BARS, THE HOLES SHALL BE THOROUGHLY CLEANED BY METHODS APPROVED BY THE ENGINEER, AFTER CLEANING OUT THE HOLES, PACK WITH AN APPROVED GROUT AND DRIVE THE REINFORCING DAY TO THE SEAT A LIST OF ADDROVED CROUTE MAY BE OBTAINED. BAR TO ITS SEAT. A LIST OF APPROVED GROUTS MAY BE OBTAINED FROM THE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS

DATE ________

DATE 9-92

DRAM RYKIM FRANKENFIELD

OFCKED BY MAINTURFE

SUPERVISED BYGENTRY & MAINTURES DATE 9-12

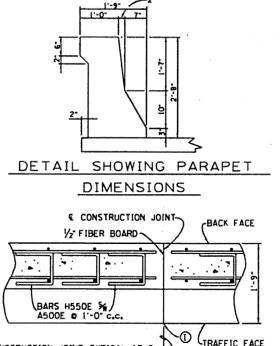
MOTE, AFTER REMOVING PORTIONS OF THE EXISTING BRIDGERAIL AND ENDPOST, AND WHEN POURING THE NEW CONCRETE PARAPET THERE MAY BE AREAS ON TOP OF THE EXISTING CURBS OR WINGWALLS WHERE REINFORCING BARS AND/OR ANCHOR BOLTS MAY BE EXPOSED. IN THESE AREAS THE BARS AND/OR ANCHOR BOLTS SHALL BE CUT I'/ MIN BELOW THE EXISTING CONCRETE SURFACES, AND THE VOID FILLED WITH AN EPOXY SEALER. ALL WORK MUST MEET WITH THE APPROVAL OF THE ENGINEER. COST SHALL BE INCLUDED IN ITEM NO. 620-10.01.

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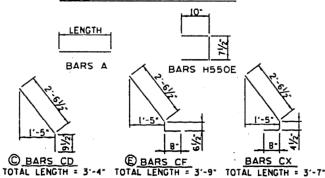
EXISTING

REINFORCING STEEL

- (F) NOTE, BARS ASOLE IN THE TRAFFIC FACE OF THE PARAPET TO REST ON TOP OF THE EXISTING CURB AND CLEAR THE TOP OF THE NEW POURED PARAPET 11/2" ±.
- © DENOTES, COST OF REMOVING ANY PORTIONS OF EXISTING ASPHALT AND REPLACING OF SAME SHALL BE INCLUDED IN COST OF ITEMS BID ON.
- (H) NOTE, FOR PARAPET DELINEATOR DETAILS AND SPACING, SEE STANDARD DRAWING SID-1-1.



BENTS AND EXISTING EXPANSION JOINTS SECTION W - W



@ BARS 'CD' ARE TO BE USED WHEN PLACING NEW CONCRETE PARAPET ACROSS BRIDGE WITH EXISTING OR NEW ASPHALT OVERLAY. (E) BARS 'CF' MAY BE SUBSTITUTED FOR BARS 'CD' ONLY WHEN PLACING NEW CONCRETE PARAPET ACROSS BRIDGE WITH NEW 41/2" CONCRETE SLAB OVERLAY.

> MOUNTAIN GREYS / WHITE NO. 36440 NO. 37886

AA TEXTURE COATING DETAIL (SHOWING LIMITS OF TEXTURE COATING FEDERAL SPECIFICATION NO.'S) COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NO. 620-10.01. AND INCLUDES 10'-2" ENDPOST AS SHOWN ON DRAWING NO. SBR-2-125.)

▲ \$ESTIMATED QUANTITIES - PER FOOT

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	ITEM		EPOXY COATED REINFORCING STEEL LB.					
	DETAILS 'A' € 'B'	0.12	19.6	1.05				
	DETAILS 'C' & 'D'	0.12	19.6	0.75				

NOTE: ESTIMATED QUANTITIES PER FOOT ARE FOR DETAILS 'A' THRU
'D' FROM BEGINNING TO END OF EXISTING BRIDGE. THE ESTIMATED
OUANTITIES FOR THE 10'-2' ENDPOST AT BRIDGE ENDS ARE SHOWN
ON DWG. NO. SBR-2-125. AND SHALL BE INCLUDED IN ITEM NO.
620-10.01. CONCRETE PARAPET (BRIDGERAIL REPLACEMENT). LIN. FT.

	PROJECT NO.		NO.	YEAR	SHEET NO.	
				1988		
	REVISIONS					
	NO. DATE BY		BRIEF DESCRIPTION			
	-	4-30-89	***	REVISED HISOE	BARS LOCATION & NOTE	
	2	5-26-89	MAN	REVISED NOTES	C (TEM NUMBER	
	3	11-15-90	-	REVISED TEXTUR	E COATING DETAIL.	
				ESTIMATED QUAN	TITIES « NOTES	
	4	11-16-92	-	REVISED DRARIN		
%	5	12-2-94	70	REVISED NOTE I	SECTION #-#	
_						
B/R	6	3-1-95	TO		AL NOTES, REMOVED	
					ADDED DETAIL OF	
				PARAPET DELIN		
B/R	7	1-4-96	10	REVISED BR DR		
				SBR DRAWING N		
			<u> </u>	REVISED STD-2	1 TO STD-1-1	

GENERAL NOTES

SPECIFICATIONS STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION, (CURRENT EDITION)

DESIGN SPECIFICATIONS, AASHTO (CURRENT EDITION) WITH ADDENDA. CONCRETE: SHALL BE CLASS 'A' F'C . 3.000 P.S.I. SEE STANDARD SPECIFICATIONS - CONCRETE STRUCTURES.

AREINFORCING STEEL: SEE STANDARD SPECIFICATIONS.

FINISHING CONCRETE SURFACES: A CLASS I FINISH FOLLOWED BY AN APPLIED

TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH, SEE

STANDARD SPECIFICATIONS, THE COLOR OF THE FINISH SHALL BE AS SHOWN
IN TEXTURE COATING DETAIL THIS SHEET, FEDERAL COLOR STANDARD NO, 595A AND A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAYING AND
HAULING OPERATIONS AT THE BRIDGE SITE.

NOTE, WHEN REMOVING PORTIONS OF THE EXISTING CONCRETE CURBING
CARE SHALL BE TAKEN SO AS NOT TO DAMAGE THE EXISTING REINFORCING
STEEL. ALL EXISTING REINFORCING STEEL IN THESE AREAS ARE TO REMAIN
PLACE AND SHALL BE COMPLETELY CLEANED TO THE FULL SATISFACTION OF THE
ENGINEER BEFORE REPOURING THE NEW CONCRETE PARAPET.

NOTE; PROFILE OF CONCRETE PARAPET AND ENDPOST SHALL CONFORM TO ROADWAY GRADE.

MOTE, DETAILS OF GUARDRAIL SHALL CONFORM TO STANDARD DRAWING S-GR-SERIES. EXCEPT FOR DETAILS SHOWN ON THIS SHEET.

E SPECIAL NOTE TO CONTRACTOR WHEN POURING NEW SLABS

THE CONTRACTOR SHALL POUR THE NEW CONCRETE SLAB TO THE CURB
REMOVAL LINE BEFORE POURING NEW PARAPET. BARS CF500E SHALL BE
LEFT PROJECTING OUT OF THE NEW SLAB.

ANDTE, TOPS OF EXISTING CURBS AND EXISTING EXPOSED REINFORCING STEEL SHALL BE BLAST CLEANED BEFORE POURING THE NEW CONCRETE PARAPETS. COSTS OF BLAST CLEANING SHALL BE INCLUDED IN ITEM NO. 620-10.01.

() NOTE: CONTRACTOR TO POUR NEW CONCRETE JERSEY SHAPE PARAPETS WITH CONSTRUCTION JOINTS AT & OF ALL BENTS AND EXISTING EXPANSION JOINTS AT BESIGNATED LOCATIONS

FOR PLACING THE CONSTRUCTION JOINTS AT DESIGNATED LOCATIONS

SHALL BE INCLUDED IN ITEM NO. 620-10.01.

NOTE, BRIDGES WITH CURB WIDTHS LESS THAN 3'-3" SHALL HAVE EXISTING BRIDGERAIL COMPLETELY REMOVED. BRIDGES WITH CURB WIDTHS 3'-3" OR WIDER SHALL HAVE ONLY THE TOP RAIL AND RAIL POST REMOVED. REINFORCING BARS IN THE POST SHALL BE CUT OFF FLUSH WITH THE TOP OF THE EXISTING CURBS. THE AREA OF POST REMOVAL SHALL BE REPOURED TO TOP OF THE BOTTOM RAIL. SEE DETAIL "X' ABOVE. COST OF REMOVING OLD RAILS AND REPOURING SECTIONS OF BOTTOM RAIL SHALL BE PAID LINDER ITEM NO. 620-10.01.

MOTE; COST OF EXISTING CONCRETE CURB REMOVAL, NEW CLASS 'A' CONCRETE.

NEW EPOXY COATED REINFORCING STEEL, FORMING, EXISTING BRIDGERAIL
REMOVAL, BLASTCLEANING, DRILLING, CONCRETE GROUTS, TEXTURED COATING,
EXCAVATION, BACKFILLING, LABOR AND ALL MISCELLANEOUS MATERIALS
NECESSARY TO BUILD THE NEW CONCRETE PARAPET WITH NEW 10'-2' CONCRETE
ENDPOST AT BRIDGE ENDS SHALL BE INCLUDED IN ITEM NO. 620-10.01.

MINOR REVISION - FHWA APPROVAL NOT REQUIRED

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPOST 1988

