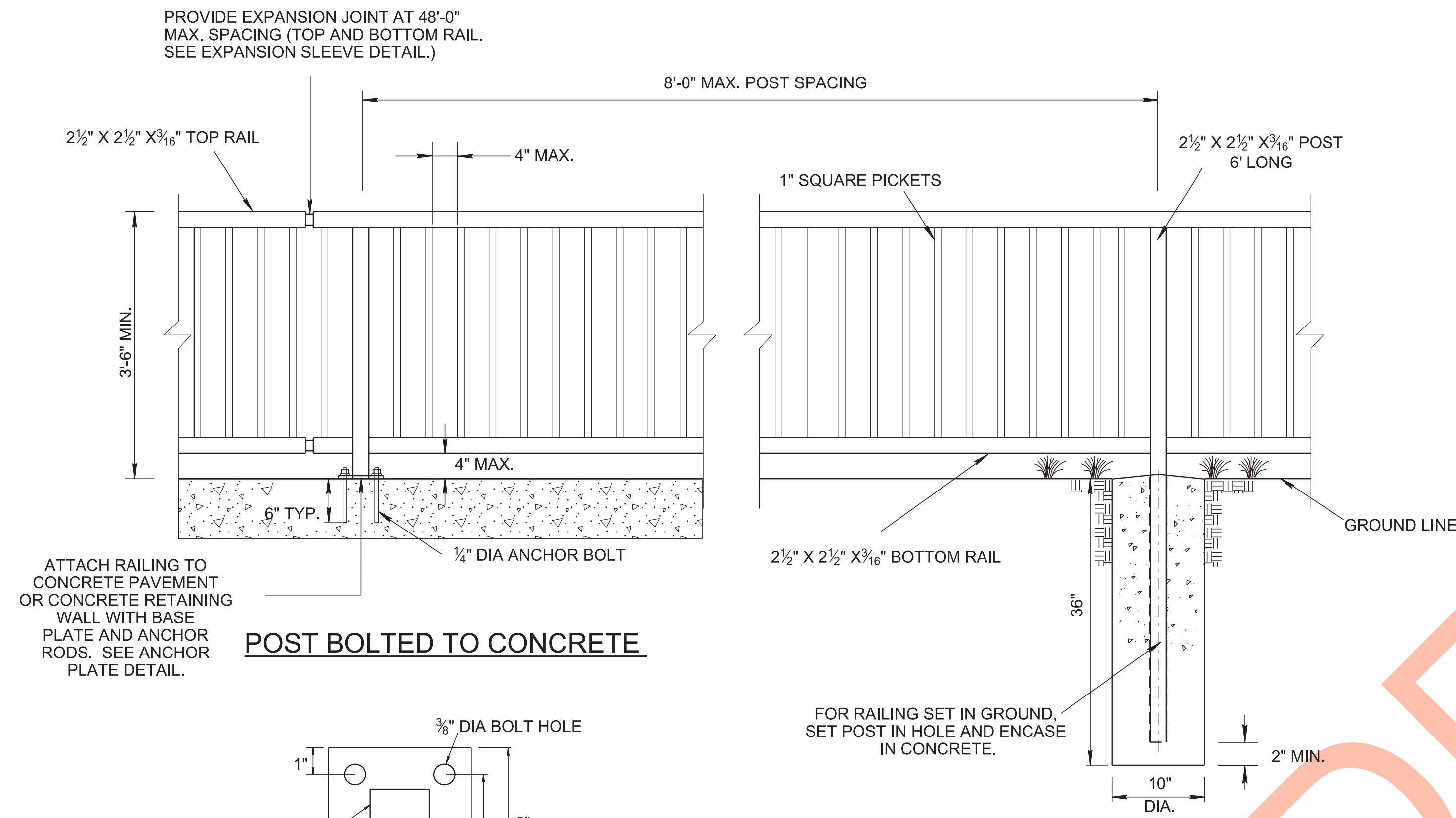
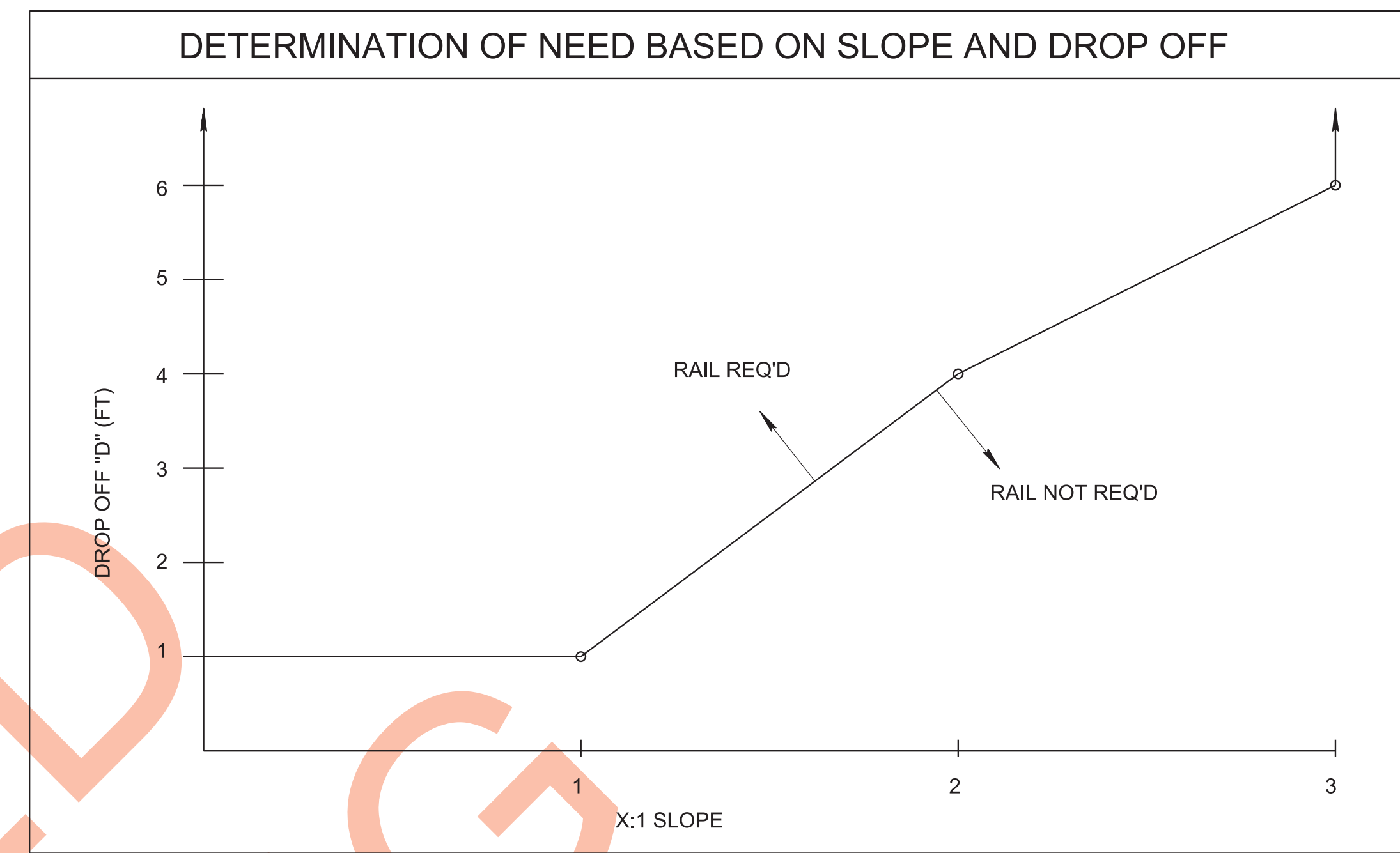


- REV. 6-4-14: CHANGED PAY ITEM NO.
- REV. 2-5-16: ADDED ALTERNATE INSTALL DETAIL.
- REV. 7-5-17: MODIFIED GENERAL NOTES, ADDED GENERAL NOTE ⑧.

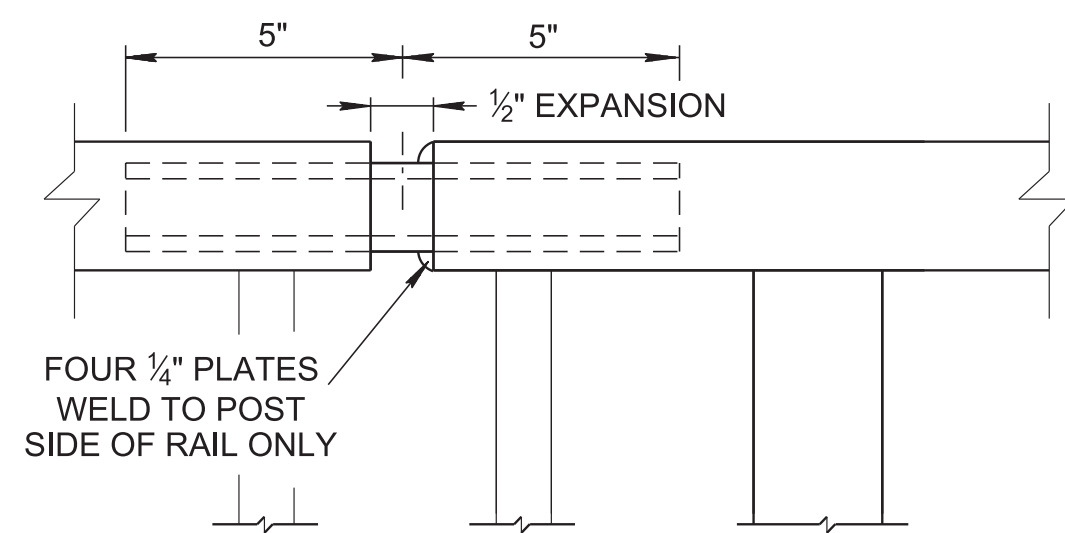
FIGURE A



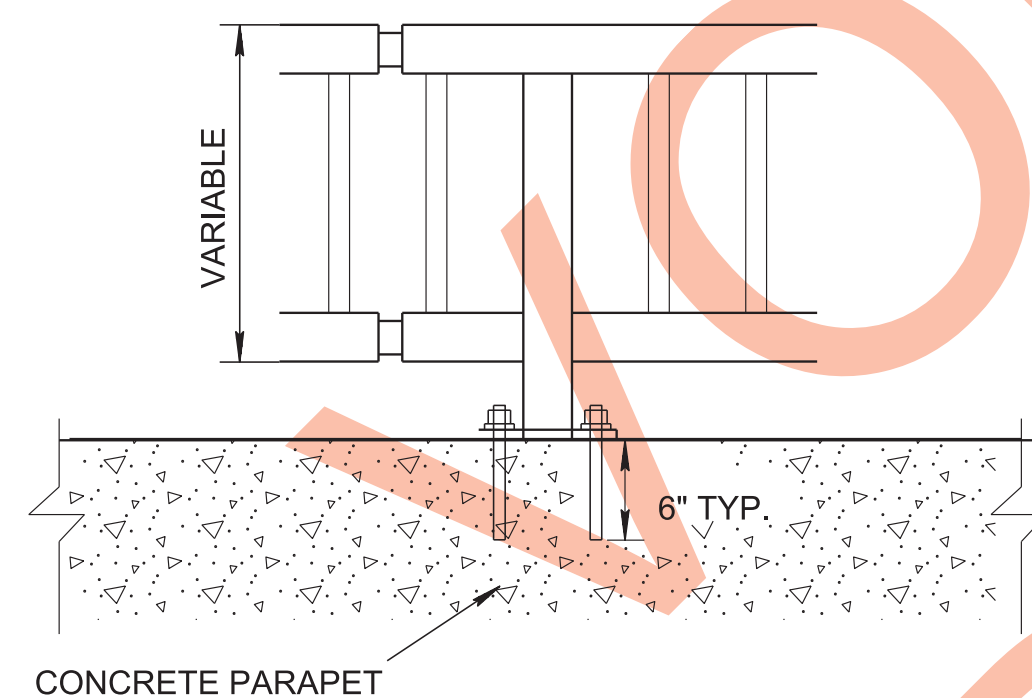
POST BOLTED TO CONCRETE

POST SET IN GROUND

ANCHOR PLATE DETAIL

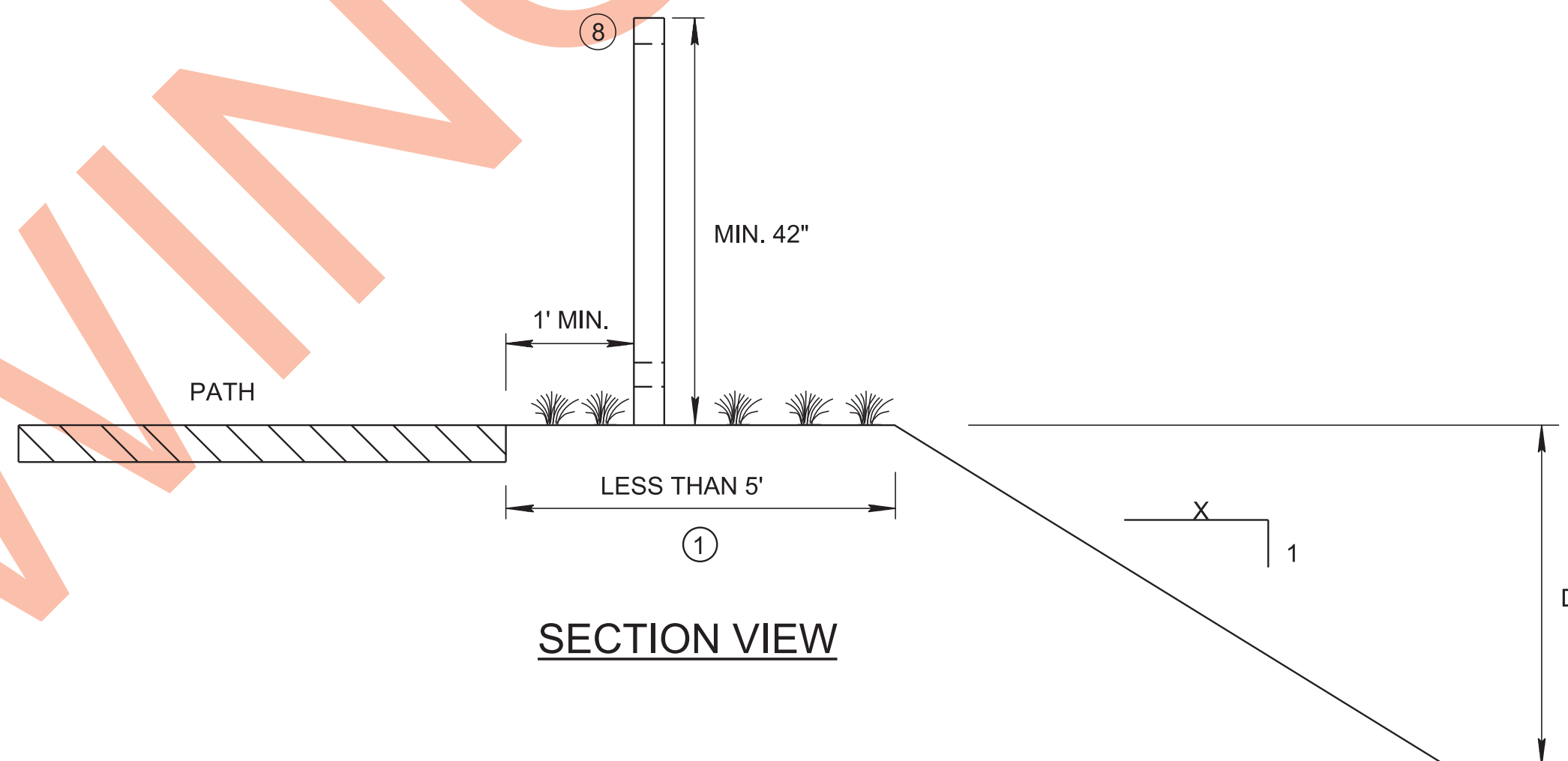


EXPANSION SLEEVE DETAIL ⑤

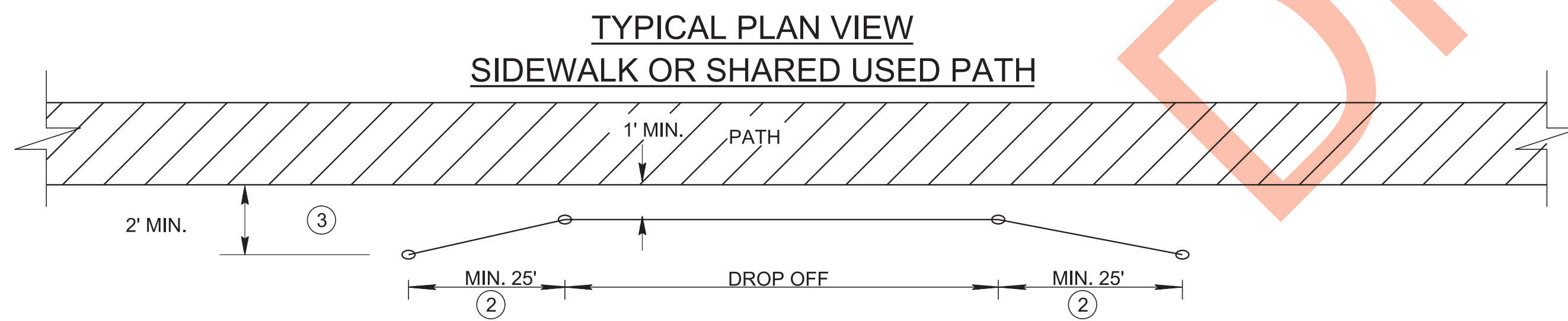


ALTERNATE INSTALLATION

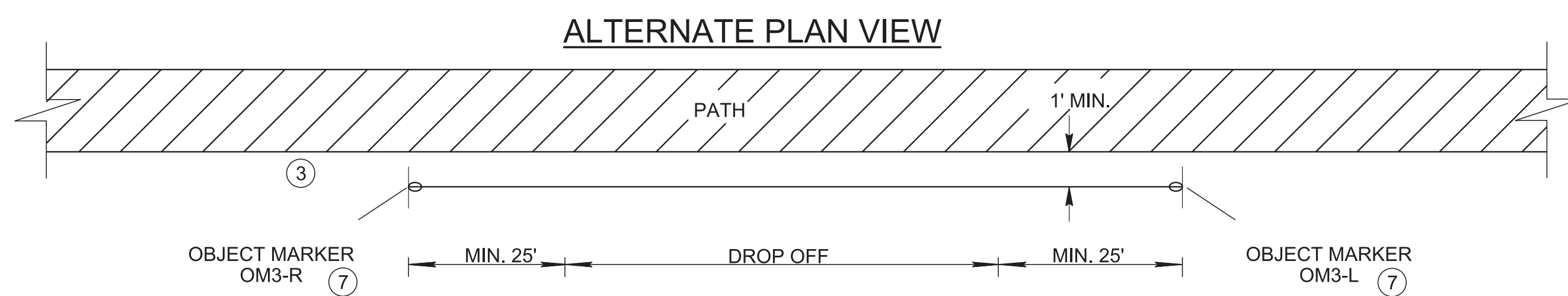
RETROFITTING EXISTING CONCRETE BARRIER TO MEET MIN 42\"/>



SECTION VIEW



TYPICAL PLAN VIEW
SIDEWALK OR SHARED USED PATH



ALTERNATE PLAN VIEW

GENERAL NOTES

- ① TO DETERMINE IF SAFETY RAIL IS REQUIRED, REFER TO FIGURE A WHEN SHARED USE PATH OR SIDEWALK EDGE DROP OFF IS WITHIN 5'. INFORMATION IS PROVIDED FOR GUIDANCE ONLY, SOME SITES MAY REQUIRE A RAIL PER ENGINEERING JUDGEMENT. IF SIDEWALK IS ALONG A ROADWAY, SEE STANDARD DRAWING S-PL-6 FOR SAFETY HARDWARE PLACEMENT.
- ② SAFETY RAIL SHALL BEGIN 25' BEFORE AND EXTEND 25' BEYOND AREA OF NEED.
- ③ SAFETY RAIL ENDS SHALL BE FLARED TO BEYOND 2' OF THE EDGE OF THE PATH OR MARKED WITH OBJECT MARKERS.
- ④ STEEL SHALL CONFORM TO ASTM A36. WELD ALL COMPONENTS USE 3#16\"/>

□ MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION

BIKE/PEDESTRIAN
SAFETY RAIL