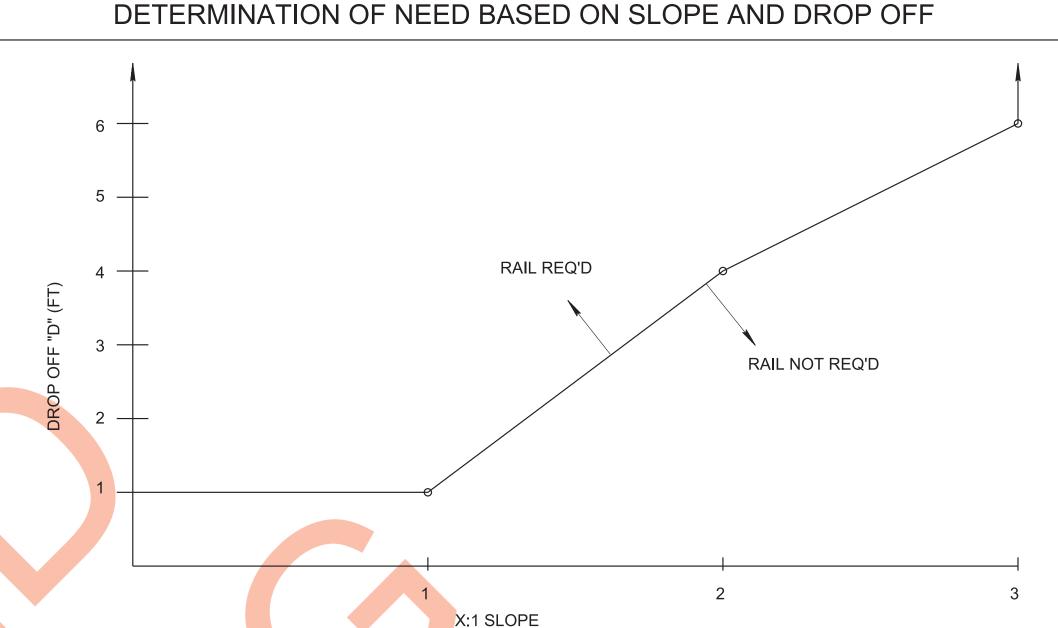
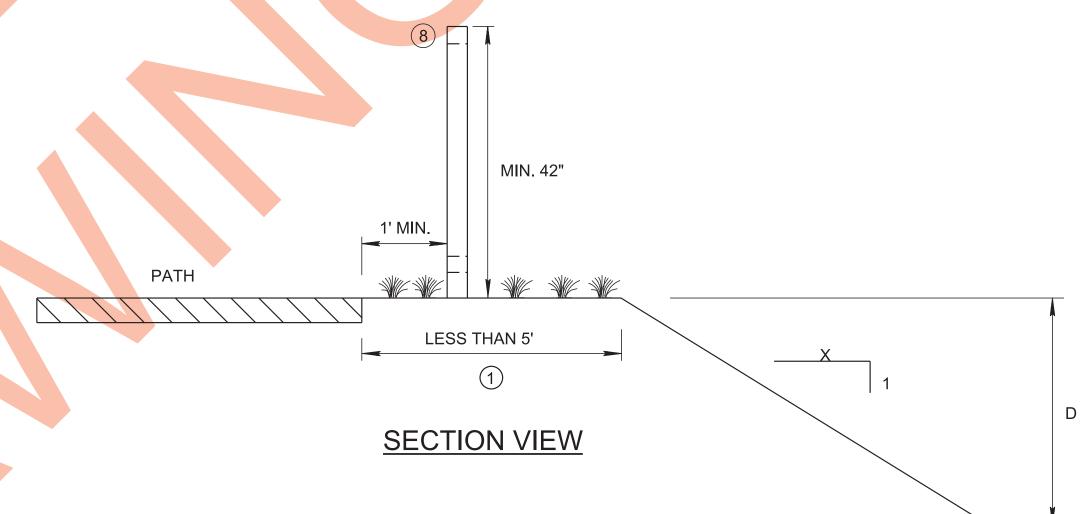


FIGURE (A)





## GENERAL NOTES

- TO DETERMINE IF SAFETY RAIL IS REQUIRED, REFER TO FIGURE (A) WHEN SHARED USE PATH OR SIDEWALK EDGE DROP OFF IS WITHIN 5'. INFORMATION IS PROVIDED FOR GUIDANCE ONLY, SOME SITES MAY REQUIRE A RAIL PER ENGINEERING JUDGEMENT. IF SIDEWALK IS ALONG A ROADWAY, SEE STANDARD DRAWING S-PL-6 FOR SAFETY HARDWARE PLACEMENT.
- 2) SAFETY RAIL SHALL BEGIN 25' BEFORE AND EXTEND 25' BEYOND AREA OF NEED.
- 3 SAFETY RAIL ENDS SHALL BE FLARED TO BEYOND 2' OF THE EDGE OF THE PATH OR MARKED WITH OBJECT MARKERS.
- STEEL SHALL CONFORM TO ASTM A36. WELD ALL COMPONENTS USE 3#16" FILLET WELDS. GRIND WELDS AND CONNECTIONS AS REQUIRED TO PROVIDE A SMOOTH SURFACE, FREE OF BURRS.

FIELD PAINT SAFETY RAIL AFTER INSTALLATION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

- (5) DETAIL SHOWN IS FOR TOP RAIL. EXPANSION JOINT FOR BOTTOM RAIL IS SIMILAR.
- 6 SYSTEM REPLACEMENTS MAY BE ALLOWED PROVIDING THAT THE HEIGHT AND SPACING LIMITATIONS SHOWN ON THIS DRAWING ARE MET.
- TO BE PAID FOR UNDER ITEM NO. 604-01.20 BOX TUBE SAFETY RAIL PER LINEAR FOOT. OBJECT MARKERS SHALL BE PAID FOR UNDER ITEM NO. 604-01.20 BOX TUBE SAFETY RAIL PER LINEAR FOOT.
- 8. STANDARD RIGHT-OF-WAY CHAIN LINK FENCE 4' IN HEIGHT MAY BE USED AS AN ALTERNATIVE TO THE BOX TUBE SAFETY RAIL AND IS TO BE PAID FOR UNDER ITEM NUMBERS:

707-01.01 CHAIN-LINK FENCE (4-FOOT) PER L.F.
707-01.02 END & CORNER POST ASSEMBLY(CHAIN-LINK FENCE 4') PER EAC

707-01.02 END & CORNER POST ASSEMBLY(CHAIN-LINK FENCE 4') PER EACH 707-01-04 GATE - CHAIN-LINK FENCE-4 FOOT (DESCRIPTION) PER EACH SEE STANDARD DRAWING S-F-10B.

MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

REV. 6-4-14: CHANGED PAY ITEM NO.

ALTERNATE INSTALL DETAIL.

GENERAL NOTES. ADDED GENERAL NOTE 8.

REV. 2-5-16: ADDED

REV. 7-5-17: MODIFIED

STATE OF TENNESSEE
DEPARTMENT OF
TRANSPORTATION

BIKE/PEDESTRIAN SAFETY RAIL

7-11-13

S-BPR-1