

REV. 2-28-98: CHANGED CHANNELIZATION DEVICE LEGEND. CHANGED LEGEND FOR FLEXIBLE DRUMS. MODIFIED HEADING AND SHEET NAME.

REV. 5-27-98: CHANGED DRAWING NO. T-WZ-10 TO T-WZ-15. ADDED GENERAL NOTES.

REV. 4-15-04: CHANGED W4-2 SIGN. CHANGED GENERAL NOTE (A) TO COMPLY WITH 2003 MUTCD.

REV. 9-1-05: REMOVED TYPE "C" WARNING LIGHTS FROM FLEXIBLE DRUMS IN TAPER.

REV. 03-13-09: MODIFIED FLEXIBLE DRUM SPACING.

REV. 03-05-17: ADDED ITEM NO. 716-05.02 AND 712-09.02.

REV. 06-28-19: ADDED TABLE FOR BUFFER SPACE. REPLACED GENERAL NOTE (A). RENAMED AND REDREW SHEET.

REV. 05-01-2020: ALTERED SIGNS TO MATCH 2009 MUTCD. ADDED SPECIAL NOTE. ADDED TMA TRUCK SPECIAL NOTE. ADDED BUFFER SPACE TABLE. REMOVED 716-05.02 PAY ITEM. CHANGED CALCULATION FOR DISTANCE L.

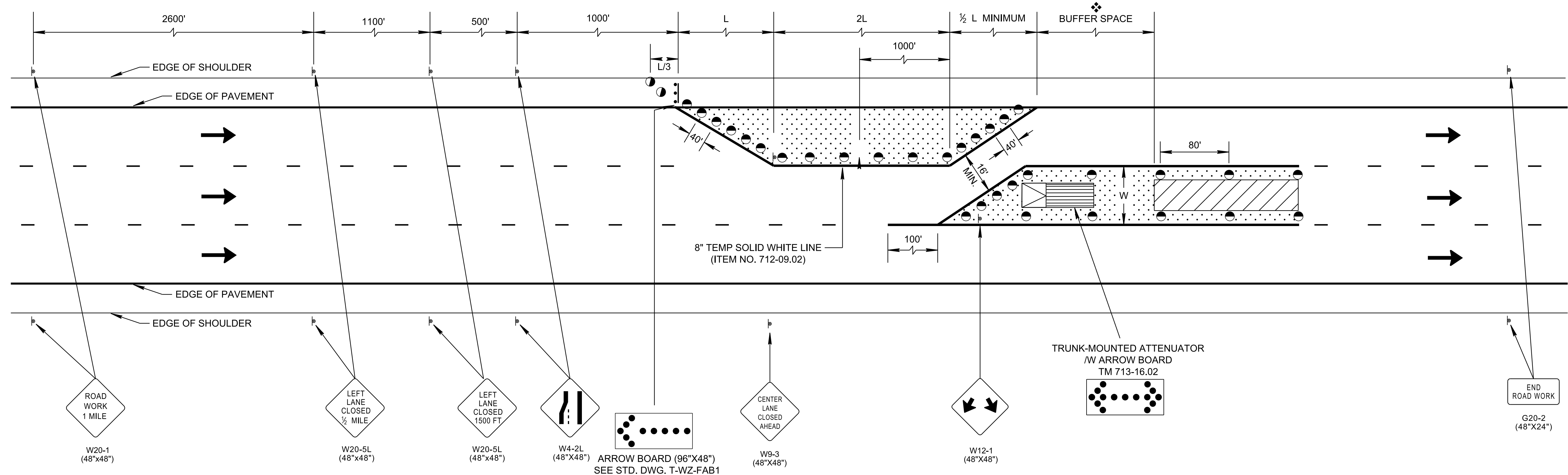
NOTE TO DESIGNER

USE OF THIS SETUP SHOULD BE CONSIDERED ONLY WHEN ABSOUTELY NECESSARY.

USE OF LAW ENFORCEMENT SHOULD BE USED WITH THIS CONFIGURATION.

BEFORE THIS STANDARD IS USED, DESIGNER SHOULD EXPLORE OTHER TRAFFIC CONTROL OPTIONS. CONSULTATION WITH ONE OF THE FOLLOWING TDOT PERSONNEL SHOULD BE CONSIDERED BEFORE IMPLEMENTING THIS TRAFFIC CONTOL CONFIGURATION.

REGIONAL TRAFFIC ENGINEER
STATE TRAFFIC ENGINEER
STATE WORK ZONE ENGINEER



TRAFFIC CONTROL FOR INTERIOR LANE CLOSURE ON FREEWAYS OR EXPRESSWAYS

❖ BUFFER SPACE
BASED ON
PRE-CONSTRUCTION
POSTED SPEED

SPEED	DISTANCE
45	360
50	425
55	495
60	570
65	645
70	730
75	820

COMPUTATION FOR DISTANCE L

$L = W \times S$

L = TAPER LENGTH IN FEET
W = WIDTH OF OFFSET IN FEET
S = 10 MPH OVER EXISTING POSTED SPEED LIMIT

CHANNELIZATION DEVICE LEGEND

- FLEXIBLE DRUMS (ITEM NO. 712-04.01, PER EACH)
- FLASHING YELLOW ARROW BOARD (SEE STD. DWG. NO. T-WZ-FAB1, FOR DETAILS AND SPECIFICATIONS) (ITEM NO. 712-08.03, PER EACH)
- SIGN SUPPORT
- DIRECTION OF TRAFFIC
- WORK SPACE
- TRUCK-MOUNTED ATTENUATOR

SPECIAL NOTES

UNLESS SPECIFIED OTHERWISE LANE WIDTHS SHALL BE A MINIMUM OF 11FT WITH 2FT SHOULDER. WHEN ALIGNMENT IS IN A HORIZONTAL CURVE WITH A RADIUS LESS THAN 2500FT, LANE WIDTHS SHALL BE A MINIMUM OF 12FT WITH 2FT SHOULDER.

IF THE MINIMUM DESIGN REQUIREMENTS OF THIS STANDARD DRAWING CANNOT BE MET, A WORK ZONE DESIGN DEVIATION MUST BE SUBMITTED TO AND APPROVED BY THE STATE WORK ZONE ENGINEER.

GENERAL NOTE

(A) THIS STANDARD CAN BE USED FOR THE CLOSURE OF A CENTER LANE ON MULTI-LANE DIVIDED FREEWAYS OR EXPRESSWAYS WHEN THE RIGHT LANE MUST REMAIN OPEN. FOR THE CLOSURE OF OTHER LANES SEE STANDARD DRAWING NOS.T-WZ-13, TWZ-14, OR T-WZ-15 FOR DETAILS.

(B) SEE STANDARD DRAWING T-WZ-10 FOR OTHER NECESSARY ADVANCE WARNING SIGNS.

APPROVED BY FHWA
(ALL OTHERS APPROVED BY TDOT)

STATE OF TENNESSEE
STANDARD DRAWING
DEPARTMENT OF TRANSPORTATION

INTERIOR LANE CLOSURE FOR FREEWAYS

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