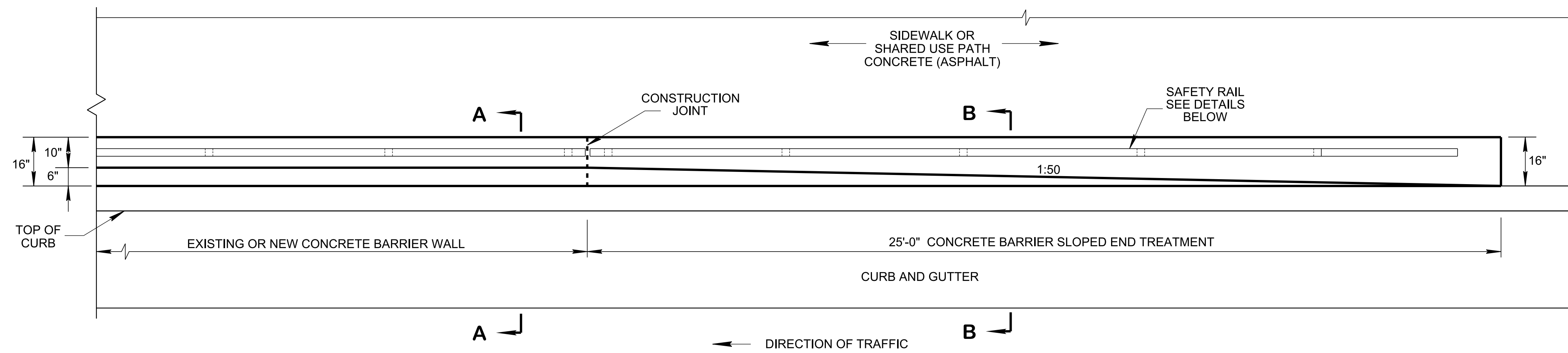
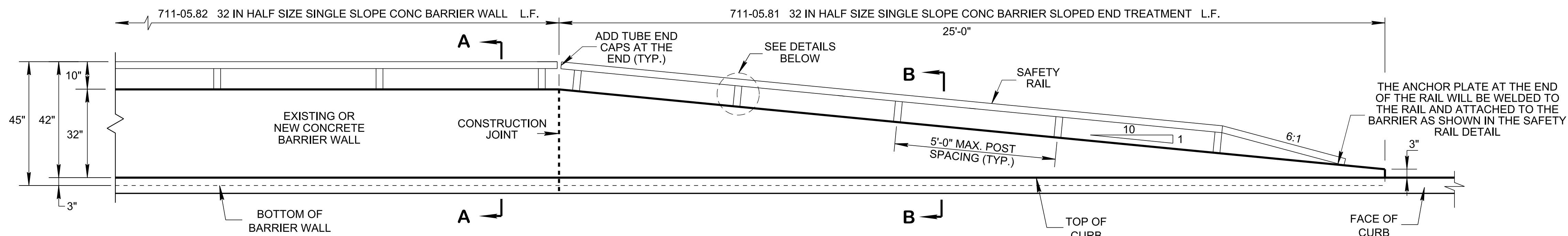


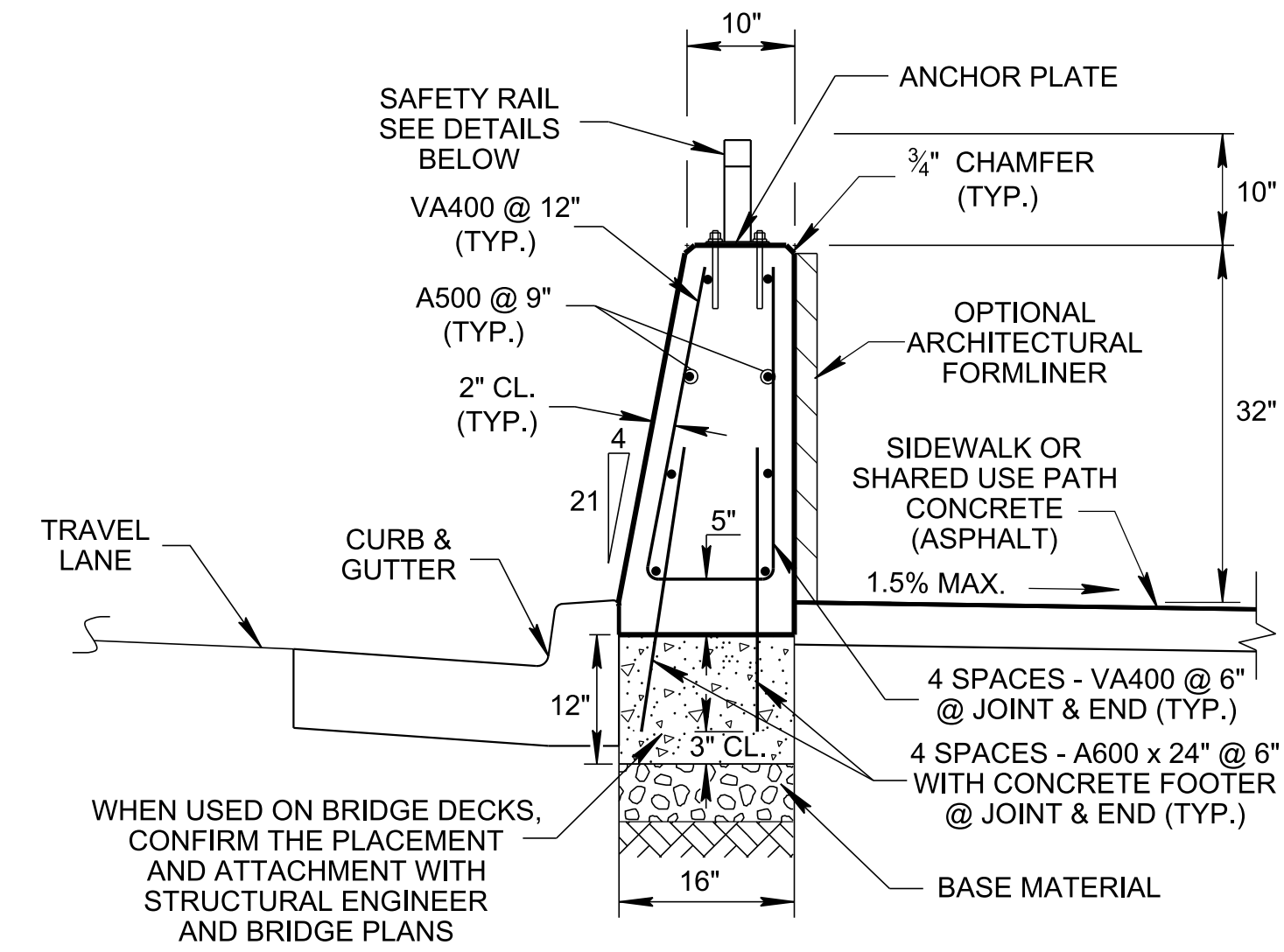
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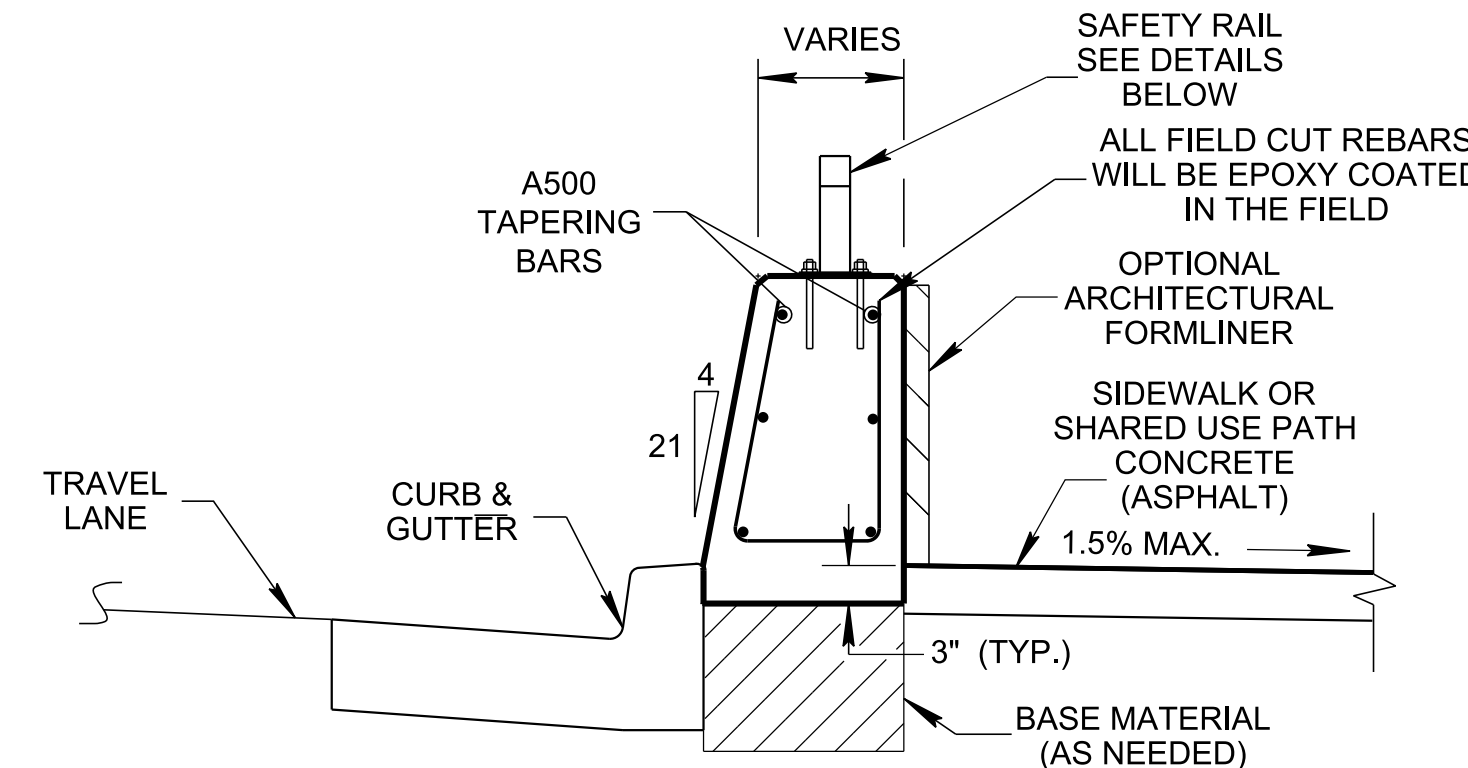
PLAN VIEW



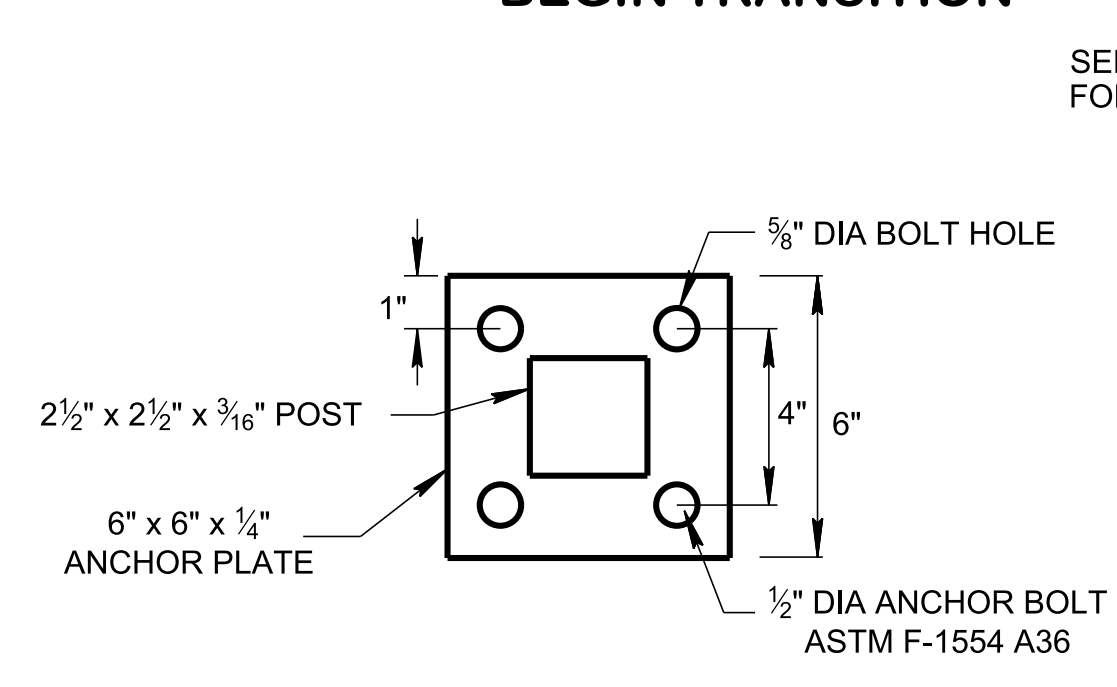
ELEVATION VIEW



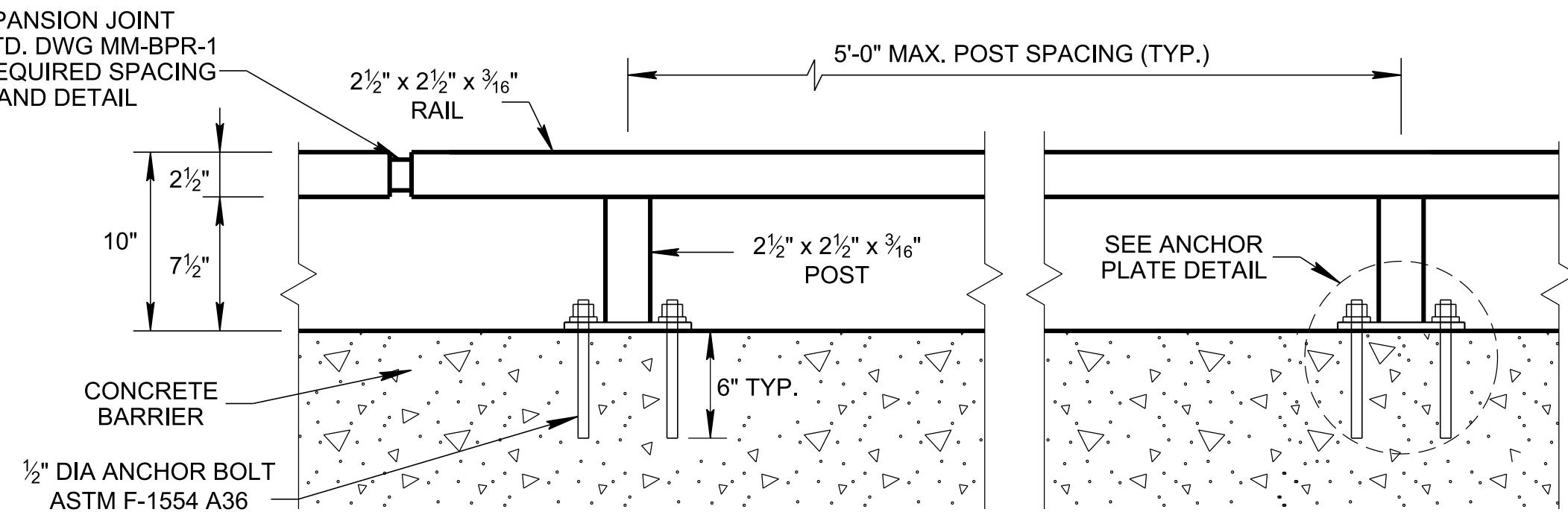
SECTION A-A
BEGIN TRANSITION



SECTION B-B
INTERMEDIATE TRANSITION



ANCHOR PLATE DETAIL



SAFETY RAIL DETAIL

GENERAL NOTES

- (A) THE EXPOSED BLUNT END OF THE HALF SIZE CONCRETE BARRIER, INSTALLED TO PROTECT VULNERABLE USERS, MAY BE PROTECTED BY THE INSTALLATION OF A SLOPED END TREATMENT TO REDUCE CRASH SEVERITY AT URBAN CONTEXT WITH SPEEDS AROUND 35 MPH.
- (B) THE USE OF A HALF SIZE CONCRETE BARRIER SHALL BE LIMITED TO LOCATIONS WITH POSTED SPEEDS LESS THAN OR EQUAL TO 45 MPH.
- (C) CONCRETE BARRIER WALL SHALL BE CONSTRUCTED IN ACCORDANCE WITH STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 711 AND/OR CURRENT SPECIAL PROVISIONS.
- (D) CONCRETE: $F'_c = 4,000$ POUNDS PER SQUARE INCH AT 28 DAYS
REINFORCING STEEL: ASTM A615, $F_y = 60,000$ POUNDS PER SQUARE INCH
ALL REINFORCING IS TO BE INSTALLED AS DETAILED ON THIS DRAWING.
- (E) REFER TO STD. DWG. MM-SERIES AND RD11-SERIES FOR ADDITIONAL DETAILS.
- (F) REHABILITATION PROJECTS MAY REQUIRE DRAINAGE STRUCTURE LOCATIONS WHERE STORM DRAINAGE DOES NOT EXISTING.
- (G) THE TOP AND END EDGES OF THE CONCRETE BARRIER WILL HAVE A 3/4" CHAMFER. ROUNDED EDGES MAY BE USED INSTEAD OF THE CHAMFER.
- (H) THE ARCHITECTURAL FORMLINER SHOWN ON THE DRAWING IS OPTIONAL. WHEN FORMLINE IS USED, THE COST FOR CONSTRUCTION OF THE FORMLINER WILL BE INCLUDED IN 32" HALF SIZE SLOPE BARRIER RAIL ITEM NUMBER.
- (I) ALL REINFORCING STEEL BARS ARE TO BE EPOXY COATED MEETING ALL REQUIREMENTS OF ASTM D3963.
- (J) SAFETY RAIL STEEL TUBES AND ANCHOR BOLTS SHALL CONFORM TO ASTM A36. WELD ALL COMPONENTS USE 3/16" FILET WELD. GRIND WELDS AND CONNECTIONS AS REQUIRED TO PROVIDE A SMOOTH SURFACE, FREE OF BURRS. SEE STD. DWG. MM-BPR-1 FOR ADDITIONAL SAFETY RAIL DETAILS.
- (K) THE CONCRETE BARRIER WALL SHALL BE GIVEN AN APPLIED TEXTURE FINISH. THE COLOR OF THE FINISH SHALL BE WHITE, FEDERAL SPECIFICATION NO. 37886. OR MATCH EXISTING CONCRETE BARRIER WALL TEXTURE. THE COST OF MATERIALS AND LABOR FOR THE TEXTURE FINISH SHALL BE INCLUDED IN THE BID PRICE FOR CONCRETE MEDIAN BARRIER.
- (L) PAYMENT:
ALL COST ASSOCIATED WITH THE SAFETY RAIL, FURNISHING, INSTALLING AND PAINTING TO BE INCLUDED IN THE COST OF CONCRETE BARRIER WALL ITEM NUMBER.
PAYMENT FOR CONCRETE BARRIER WILL BE MADE UNDER PAY ITEM NUMBER:

711-05.81 32 IN HALF SIZE SINGLE SLOPE CONCRETE BARRIER WALL L.F.
711-05.82 32 IN HALF SIZE SINGLE SLOPE CONC BARRIER SLOPED END TREATMENT L.F.

STATE OF TENNESSEE
STANDARD DRAWING
DEPARTMENT OF TRANSPORTATION

32 IN HALF SIZE SINGLE SLOPE BARRIER WALL & SLOPED END TREATMENT

NOT TO SCALE