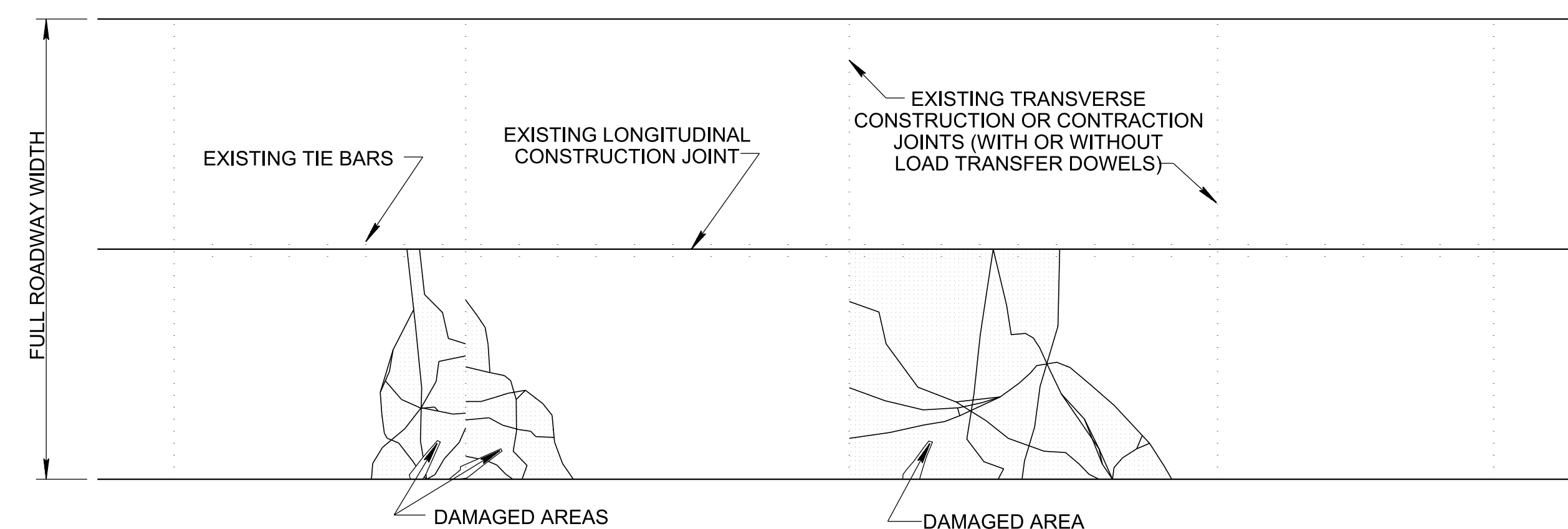
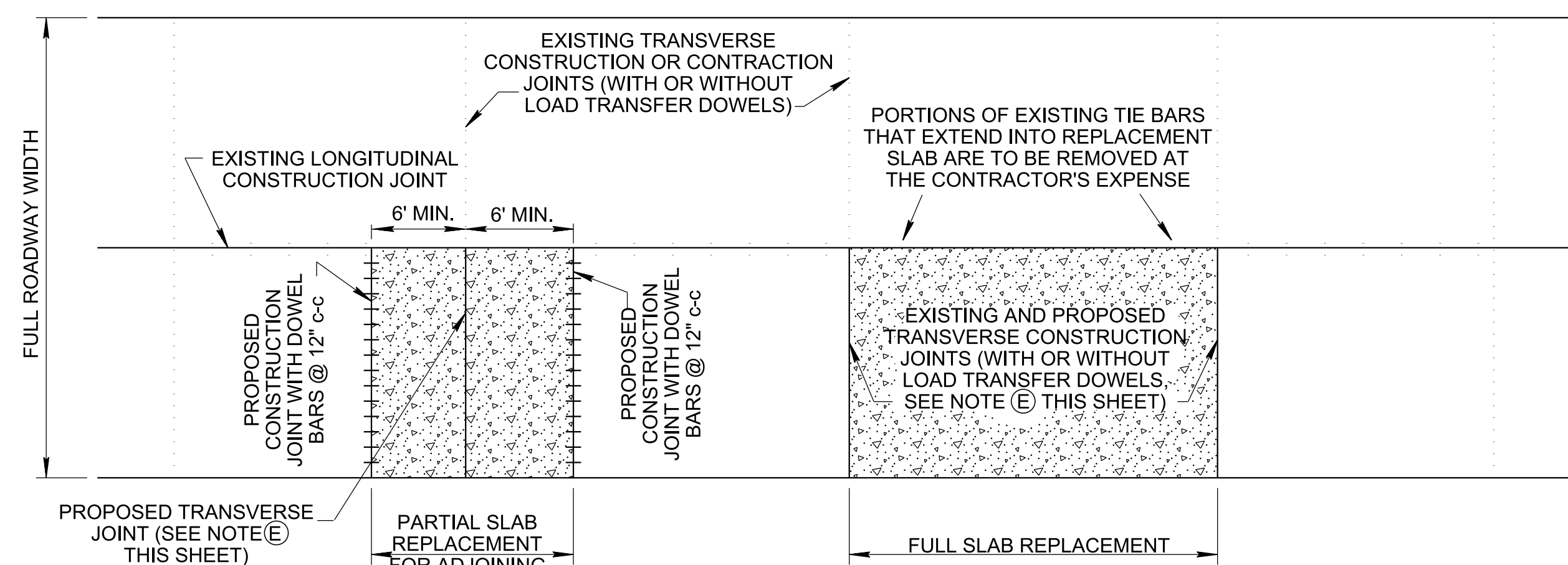


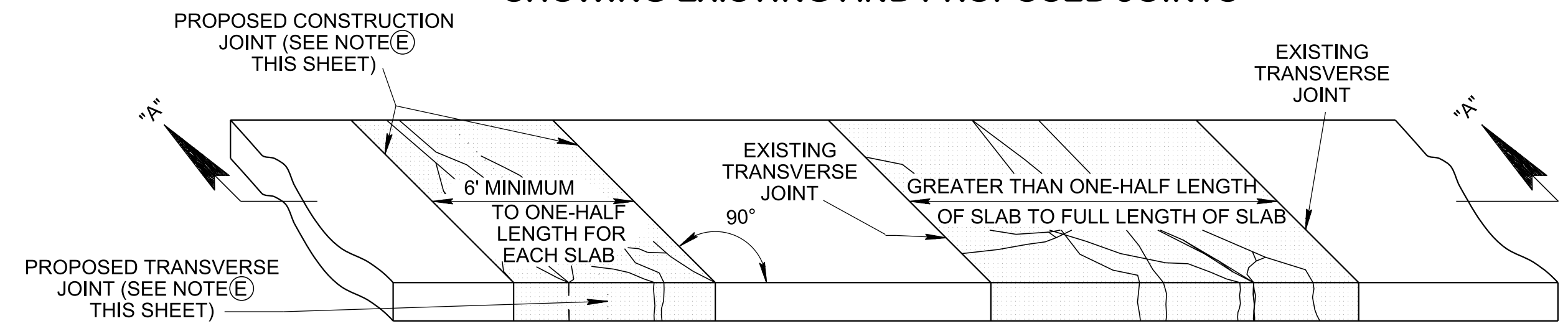
1/27/2022 2:33:18 PM P:\StandDraw\DESIGN STANDARDS\Standards Drawings\Standard Roadway Drawings - CURRENT\In Progress\10-104.00 Roadway, Pavement Appurtenances and Fence IP\104.01 Concrete Pavement IP



PLAN VIEW OF EXISTING LAYOUT OF CONCRETE PAVEMENT REPLACEMENT SHOWING EXISTING JOINTS

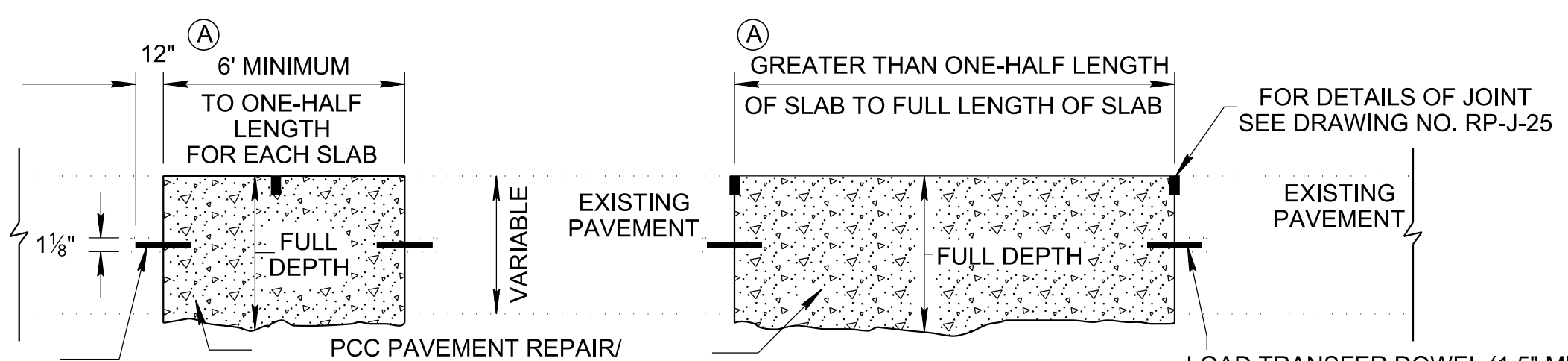


PLAN VIEW OF PROPOSED LAYOUT OF CONCRETE PAVEMENT REPLACEMENT SHOWING EXISTING AND PROPOSED JOINTS



PLAN VIEW OF CONCRETE PAVEMENT REPLACEMENT

TRANSVERSE DOWEL BARS SHALL BE NO.8 ROUND DEFORMED STEEL, 24" LONG SET 12" INTO EXISTING CONCRETE SLAB AT 12" CENTER-TO-CENTER SPACING BETWEEN DOWEL BARS. DOWEL BARS SHALL CONFORM TO ASTM A615-GRADE 40 SPECIFICATIONS.



PROFILE VIEW ALONG SECTION "A-A" OF CONCRETE PAVEMENT REPLACEMENT

NOTE

IF REPLACEMENT IS MID-SLAB, NO TRANSVERSE JOINT IS REQUIRED. IN THIS SITUATION A CONSTRUCTION JOINT WITH TIE BARS WILL BE USED.

GENERAL NOTES

- (A) SEE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL REQUIREMENTS FOR CONCRETE PAVEMENT REPAIR.
- (B) IF THE LENGTH OF CONCRETE SLAB TO BE REPLACED IS GREATER THAN HALF THE ENTIRE LENGTH OF THE SLAB, THE ENTIRE SLAB SHALL BE REPLACED. IF THE LENGTH OF CONCRETE SLAB TO BE REPLACED IS LESS THAN HALF THE ENTIRE LENGTH OF THE SLAB (6' MAX.), THEN ONLY A PORTION OF THE SLAB WILL BE REPLACED.
- (C) THE EXISTING CONCRETE PAVEMENT SHALL BE SAWED FULL DEPTH AROUND THE AREA TO BE REMOVED. WITHIN THE LANE SAWING SHALL BE PERPENDICULAR TO THE CENTERLINE AND A MINIMUM OF 6" OUTSIDE THE DAMAGED AREAS.
- (D) NO ADDITIONAL BASE MATERIAL SHALL BE ADDED AND ALL LOOSE BASE MATERIAL NOT RECOMPACTABLE SHALL BE REMOVED PRIOR TO PLACEMENT OF THE NEW CONCRETE SLAB. THE CONCRETE SLAB SHALL BE PLACED TO THE FULL DEPTH OF THE MATERIAL REMOVED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ADDITIONAL CONCRETE REQUIRED TO BRING PROPOSED CONCRETE SLAB UP TO PROPOSED GRADE.
- (E) WHEN EXISTING TRANSVERSE JOINTS ARE REMOVED AND NOT TO FULL ROADWAY WIDTH, THEY SHALL BE RECONSTRUCTED IN KIND (WITH OR WITHOUT LOAD TRANSFER DOWELS) AND IN THE SAME LOCATION. WHEN A JOINT IS REPLACED FOR THE FULL ROADWAY WIDTH, LOAD TRANSFER DOWELS SHALL BE USED IN THE JOINT. SEE DRAWING NO. RP-J-9 FOR DOWEL PLACEMENT DETAILS. SPACING IS AT 12" CENTER-TO-CENTER BETWEEN DOWELS.
- (F) FOR DETAILS REGARDING INSTALLATION OF CONTRACTION AND CONSTRUCTION JOINTS, SEE DRAWING NO. RP-J-9.
- (G) LONGITUDINAL CONSTRUCTION JOINT TIE BARS AS SHOWN ON DRAWING NO. RP-J-15 SHALL BE OMITTED BETWEEN THE NEW REPLACEMENT SLAB AND THE EXISTING SLAB. THE CONTRACTOR IS TO REMOVE WHATEVER PORTION OF THE EXISTING TIE BARS THAT EXTENDS FROM EXISTING SLAB ALONG LONGITUDINAL JOINT INTO NEW SLAB. ALL COST WILL BE INCLUDED IN THE PRICE BID FOR PCC PAVEMENT REPAIR.
- (H) REMOVAL OF THE DAMAGED CONCRETE PAVEMENT SHALL BE BY LIFTING. ANY GOOD CONCRETE PAVEMENT WHICH IS DAMAGED DURING REMOVAL OF DAMAGED AREAS SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR, AT HIS EXPENSE.
- (I) IF THE ROADWAY CONTRACT INCLUDES EITHER GRINDING OR UNDERSEALING, THEN THE SLAB REPAIR SHALL BE PERFORMED FIRST.
- (J) THE COSTS OF REMOVAL AND DISPOSAL OF EXISTING CONCRETE PAVEMENT, PLACEMENT OF NEW CONCRETE PAVEMENT, AND SAWING NEW JOINTS SHALL BE INCLUDED IN THE PRICE BID FOR THE PAY ITEM NUMBERS 502-03.XX SERIES LISTED ON THIS STANDARD DRAWING.
- (K) ONCE THE CONTRACTOR BEGINS REMOVING AN EXISTING FULL OR PARTIAL DEPTH CONCRETE SLAB, THEY SHALL CONTINUE THE WORK UNTIL IT IS COMPLETE INCLUDING JOINT SEALING. JOINTS SHALL NOT BE LEFT UNSEALED DURING WINTER MONTHS.
- (L) PAYMENT FOR THE FULL DEPTH AND PARTIAL DEPTH CONCRETE PAVEMENT REPAIR SHALL INCLUDE THE COST OF ALL RELATED WORK (DRILLING HOLES, GROUTING, ETC.) AND SHALL BE PAID FOR UNDER THE FOLLOWING ITEM NUMBERS:

502-03.20, FULL DEPTH PCC PAVEMENT REPAIR,	C.Y.,	
502-03.21, PARTIAL DEPTH PCC PAVEMENT REPAIR,	S.Y.,	
502-04.01, SAWING CONCRETE PAVEMENT (FULL DEPTH),	L.F.,	
502-04.02, LOAD TRANSFER DOWELS,	EACH,	
502-04.03, TRANSVERSE DOWEL BARS,	EACH,	
- (M) PAYMENT FOR THE FULL DEPTH AND PARTIAL DEPTH CONCRETE PAVEMENT REPAIR (CLASS X-HIGH EARLY STRENGTH) SHALL INCLUDE THE COST OF ALL RELATED WORK (DRILLING HOLES, GROUTING, ETC.) AND SHALL BE PAID FOR UNDER THE FOLLOWING ITEM NUMBERS:

502-03.25, FULL DEPTH PCC PAVEMENT REPAIR HIGH EARLY,	C.Y.,	
502-03.26, PARTIAL DEPTH PCC PAVEMENT REPAIR HIGH EARLY,	S.Y.,	
502-04.01, SAWING CONCRETE PAVEMENT (FULL DEPTH),	L.F.,	
502-04.02, LOAD TRANSFER DOWELS,	EACH,	
502-04.03, TRANSVERSE DOWEL BARS,	EACH,	
- (N) FOR FULL SLAB REPLACEMENTS ON SLABS WITH JOINT SPACING LONGER THAN 15', THE SLAB SHALL BE REPLACED WITH TWO SLABS OF EQUAL LENGTH.

- REV. 7-17-84: ADDED EXISTING AND PROPOSED LAYOUTS OF CONCRETE PAVEMENT REPLACEMENT. ADDED TIE BARS AND CHANGED NOTES.
- REV. 4-2-90: REDREW AND RENAMED SHEET. PLACED SPALL REPAIR, RANDOM CRACK REPAIR, AND JOINT REPAIR, AND JOINT REPAIRS DETAILS ON NEW SHEET NO. RP-J-24.
- REV. 12-18-94: ELIMINATED USE OF TIE BARS BETWEEN REPLACEMENT AND EXISTING SLAB.
- REV. 5-27-96: CHANGED MINIMUM SIZE OF LOAD TRANSFER DOWEL TO 1.5".
- REV. 7-29-96: CHANGED GENERAL NOTES (3) AND (8).
- REV. 5-27-01: CHANGED ITEM NO. 501-04.03.
- REV. 1-19-02: IN GENERAL NOTE 9 REMOVED REFERENCE TO UNDERSEALING OF SLAB.
- REV. 10-26-04: CHANGED PAY ITEMS IN GENERAL NOTE (2).
- REV. 1-24-12: ADDED GENERAL NOTE (13).
- REV. 7-25-12: ADDED GENERAL NOTE (14).
- REV. 05-01-20: CONVERTED GENERAL NOTE NOS. TO LETTERS AND REVISED NOTE (5) AND (M). REDREW SHEET.
- REV. 01-28-2022: REMOVED PCC (PLAIN) AND FAST TRACK ITEM NUMBERS 501-01 SERIES FROM GENERAL NOTES (C) AND (M). REVISED GENERAL NOTE (J). REVISED GENERAL NOTES (L) AND (M), AND ADDED NEW ITEM NUMBERS.

CROSS-REFERENCE DRAWINGS FOR THIS SHEET: RP-J-9, RP-J-24, AND RP-J-25.

APPROVED BY FHWA (ALL OTHERS APPROVED BY TDOT)

STATE OF TENNESSEE
STANDARD DRAWING
DEPARTMENT OF TRANSPORTATION

CONCRETE PAVEMENT REPAIR DETAILS