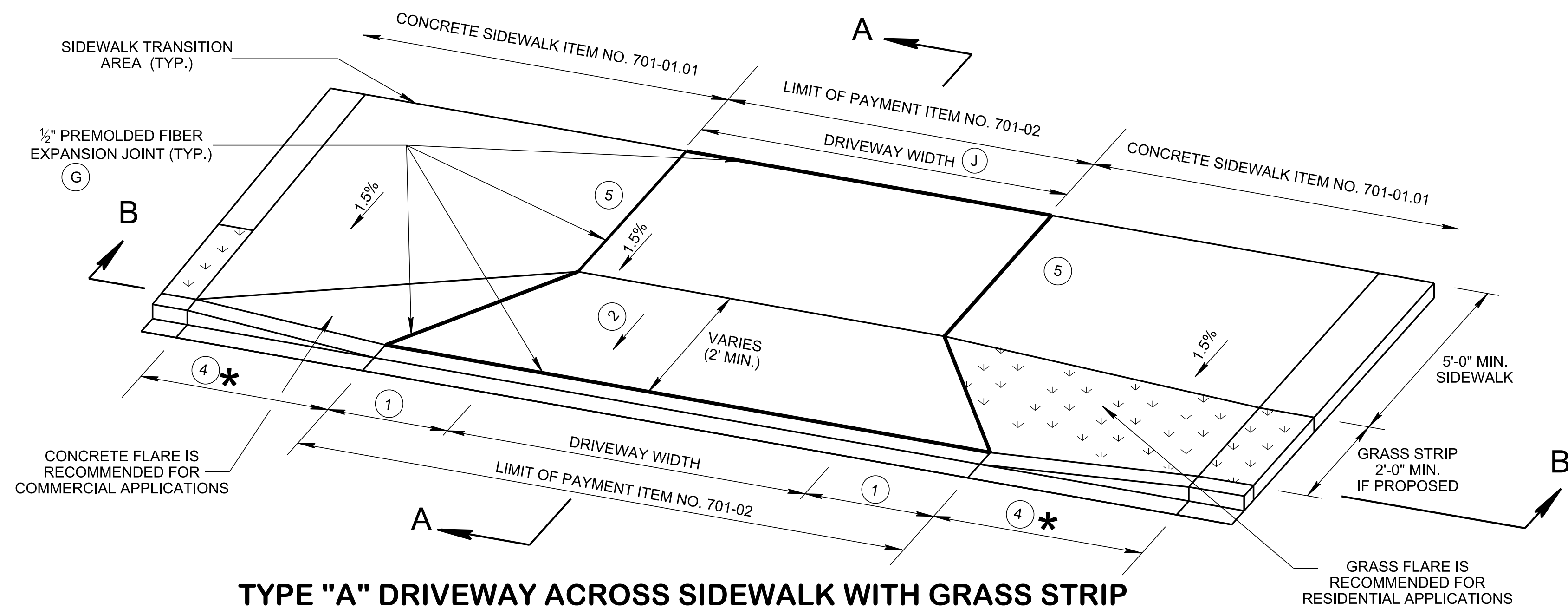


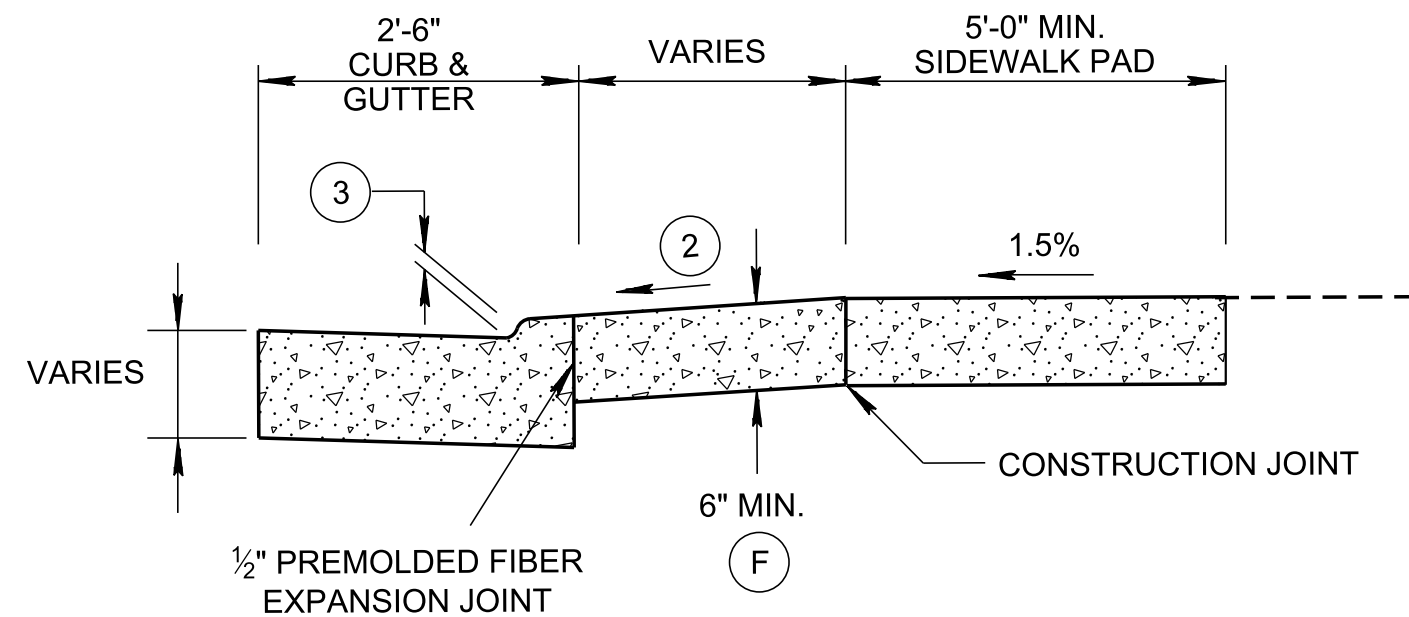
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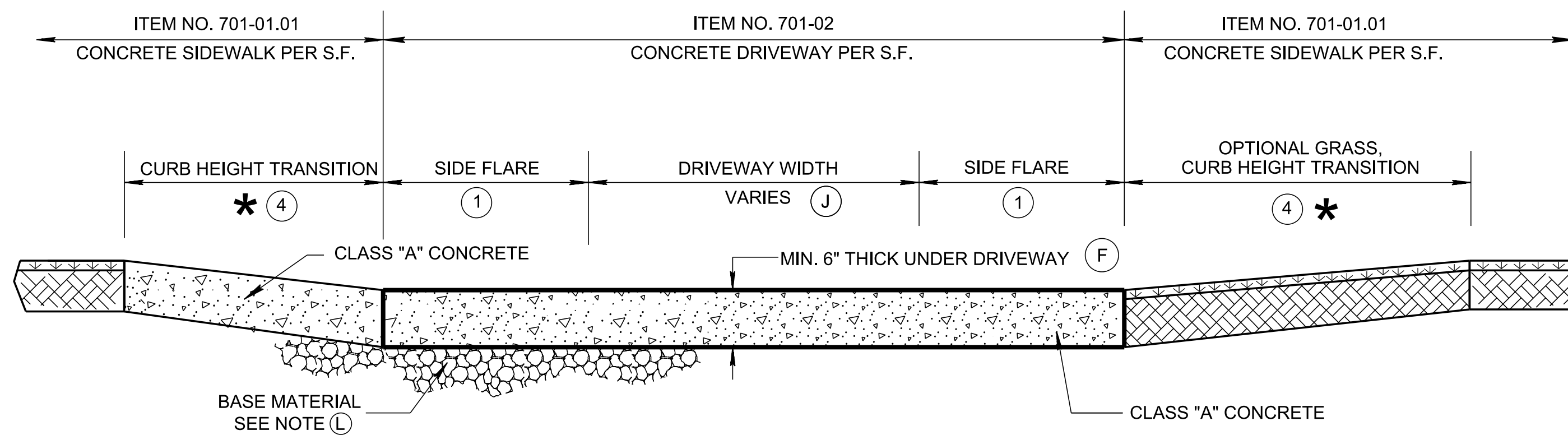
TYPE "A" DRIVEWAY ACROSS SIDEWALK WITH GRASS STRIP

LEGEND

* DIMENSION VARIES RELATIVE TO LONGITUDINAL ROADWAY GRADE.



SECTION A-A



SECTION B-B

FOOTNOTES	
①	SIDE FLARE WIDTH SHOULD BE A MINIMUM 7'-0" FOR COMMERCIAL DRIVEWAYS. SIDE FLARE WIDTH SHOULD BE A MINIMUM 3'-6" FOR RESIDENTIAL DRIVEWAYS.
②	DRIVEWAY RAMP GRADE VARIES, 15% MAX. (10% RECOMMENDED) APRON GRADE FOR RESIDENTIAL DRIVEWAYS. 8% MAX. (5% RECOMMENDED) APRON GRADE FOR COMMERCIAL DRIVEWAYS.
③	HEIGHT OF LOWERED CURB SHALL BE 2.25 INCHES. SEE STD DWG RP-VC-10 & RP-VC-11.
④	THE SLOPE OF THE SIDEWALK AND/OR CURB HEIGHT TRANSITION VARIES TO A MAXIMUM OF 8.33% LENGTH OF TRANSITION IS RELATIVE TO THE LONGITUDINAL ROADWAY GRADE.
⑤	COMMERCIAL DRIVEWAY ENTRANCE TYPICALLY (MAX. 40' WIDE) MAY REQUIRE DETECTABLE WARNING SURFACES IF ENTRANCE SERVES MORE THAN 400 VEHICLES PER DAY. SEE STD. DWG. NOS. MM-CR- SERIES FOR DETAILS.
⑥	3R PROJECTS MAY REQUIRE SLOPE CORRECTION, PARALLEL CROSS-WALK MARKINGS (ESPECIALLY AT TWO WAY DRIVEWAY ENTRANCES), AND DETECTABLE DOME SURFACE TO MAINTAIN CONTINUITY AT COMMERCIAL DRIVE ENTRANCES. ADDITIONAL SIGNS (WATCH FOR PED) MAY BE ADDED AT DRIVEWAYS BY THE DIRECTION OF AN ENGINEER IF NEEDED.

GENERAL NOTES																
(A)	THIS TYPE OF DRIVEWAY IS PREFERRED OVER THE LOWERED TYPE AS SHOWN ON RP-D-16 BECAUSE THE ELEVATION OF THE SIDEWALK REMAINS A CONSTANT FOR PEDESTRIANS.															
(B)	5'-0" MINIMUM SIDEWALK WITH A MAXIMUM CROSS SLOPE OF 1.5% THROUGH DRIVEWAYS.															
(C)	DESIGNER TO CHECK GUTTER FLOW DEPTH AT DRIVEWAY LOCATIONS TO ASSURE THAT THE DESIGN FLOW DOES NOT OVERTOP THE SIDEWALK AREA. IF OVERTOPPING OCCURS, PLACE AN INLET AT THE UPSTREAM SIDE OF THE DRIVEWAY OR PERFORM OTHER DESIGN MITIGATION.															
(D)	THE SLOPE OF THE LANDING AREA SHALL NOT EXCEED 1.5% IN THE SIDEWALK AREA.															
(E)	DRIVEWAYS TO BE BUILT COMPLETE OR IN PART AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.															
(F)	ALL DRIVEWAYS TO BE 6" UNIFORM THICKNESS, UNLESS OTHERWISE SHOWN ON PLANS.															
(G)	EXPANSION JOINTS ARE TO BE PLACED AS INDICATED ON THE PLANS. WHEN THE BACK OF THE DRIVEWAY ABUTS AGAINST A CONCRETE DRIVEWAY OR BUILDING, AN ADDITIONAL EXPANSION JOINT WILL BE PLACED AT THAT LOCATION.															
(H)	THE ROADWAY DESIGNER SHALL CONSIDER THE USE OF A CATCH BASIN ON EITHER SIDE OF THE DRIVEWAY. CAREFUL CONSIDERATION TO THE PLACEMENT OF CATCH BASINS SHALL BE TAKEN IF THE DRIVEWAY IS IN A VERTICAL SAG CURVE.															
(I)	PAY ITEMS: <table border="0" style="margin-left: 20px;"> <tr> <td>ITEM NO: 303-01,</td> <td>MINERAL AGGREGATE, TYPE A BASE, GRADING D,</td> <td>PER TON.</td> </tr> <tr> <td>ITEM NO: 701-01.01,</td> <td>CONCRETE SIDEWALK (4"),</td> <td>PER S.F.</td> </tr> <tr> <td>ITEM NO: 701-02,</td> <td>CONCRETE DRIVEWAY,</td> <td>PER S.F.</td> </tr> <tr> <td>ITEM NO: 701-02.02,</td> <td>CONCRETE DRIVEWAY (8"),</td> <td>PER S.F.</td> </tr> <tr> <td>ITEM NO: 702-03,</td> <td>CONCRETE COMBINED CURB AND GUTTER,</td> <td>PER C.Y.</td> </tr> </table>	ITEM NO: 303-01,	MINERAL AGGREGATE, TYPE A BASE, GRADING D,	PER TON.	ITEM NO: 701-01.01,	CONCRETE SIDEWALK (4"),	PER S.F.	ITEM NO: 701-02,	CONCRETE DRIVEWAY,	PER S.F.	ITEM NO: 701-02.02,	CONCRETE DRIVEWAY (8"),	PER S.F.	ITEM NO: 702-03,	CONCRETE COMBINED CURB AND GUTTER,	PER C.Y.
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(J)	TYPICAL DRIVEWAY WIDTHS ARE 12' (14' TWO WAY) FOR RESIDENTIAL AND 24' (40' MAX.) FOR COMMERCIAL. REFER TO SECTION 5 (ACCESS DESIGN) IN THE MANUAL FOR CONSTRUCTING DRIVEWAY ENTRANCES ON STATE HIGHWAY (2015).															
(K)	ALL SIDEWALKS SHALL HAVE A MINIMUM CONCRETE THICKNESS OF 4". THE SIDEWALK TRANSITION THICKNESS IS DEPENDENT UPON THE DRIVEWAY AND SIDEWALK THICKNESSES. THE COST OF THE SIDEWALK TRANSITION WILL BE INCLUDED IN THE PAY ITEM NO. OF 701-01.01.															
(L)	MINIMUM 4" MINERAL AGGREGATE BASE MATERIAL ITEM NO. 303-01 SHALL BE INSTALLED UNDER NEW CONCRETE DRIVEWAYS. SITE SPECIFIC PAVEMENT DESIGN MAY BE REQUIRED FOR COMMERCIAL DRIVEWAYS USED AS A DELIVERY ACCESS AS WELL. A DRIVEWAY PAVEMENT DESIGN WITH 6" CONCRETE PAVEMENT AND 4" AGGREGATE DEPTH MAY BE LIMITED TO LIGHT COMMERCIAL VEHICULAR TRAFFIC.															

REV. 7-15-08: UPDATED SIDEWALK DIMENSIONS.

REV. 4-8-16: ADDED ITEM NUMBERS. UPDATED SLOPES AND DIMENSIONS. UPDATED NOTES.

REV. 07-16-18: ADDED NOTES TO CONC. FLARE AND GRASS FLARE IN ISOMETRIC VIEW. ADDED GENERAL NOTE (K). CHANGED REFERENCED STD. DWG. FROM RP-NMC-10 TO RP-VC-10. ADDED NOTE (A) AND RENUMBERED THE REST. ADDED SPECIAL NOTE. REDREW SHEET.

REV. 01-07-19: CORRECTED SPELLING. REDREW SHEET.

REV. 10-16-20: ADDED GENERAL NOTE (M). ADDED MINERAL AGGREGATE ITEM NUMBER AND REFERENCE NOTE ON SECTION B-B.

REV. 06-15-21: REVISED AND MERGED GENERAL NOTES (J) AND (K). ADJUSTED LOCATION OF GENERAL NOTE NO'S. REVISED GENERAL NOTES (C), (K) AND (L). ADDED PAY ITEM NO'S. 701-01.01, 701-02.02 AND 702-03.

APPROVED BY FHWA
(ALL OTHERS APPROVED BY TDOT)

STATE OF TENNESSEE
STANDARD DRAWING
DEPARTMENT OF TRANSPORTATION

DETAILS OF
STANDARD
CONCRETE
DRIVEWAYS