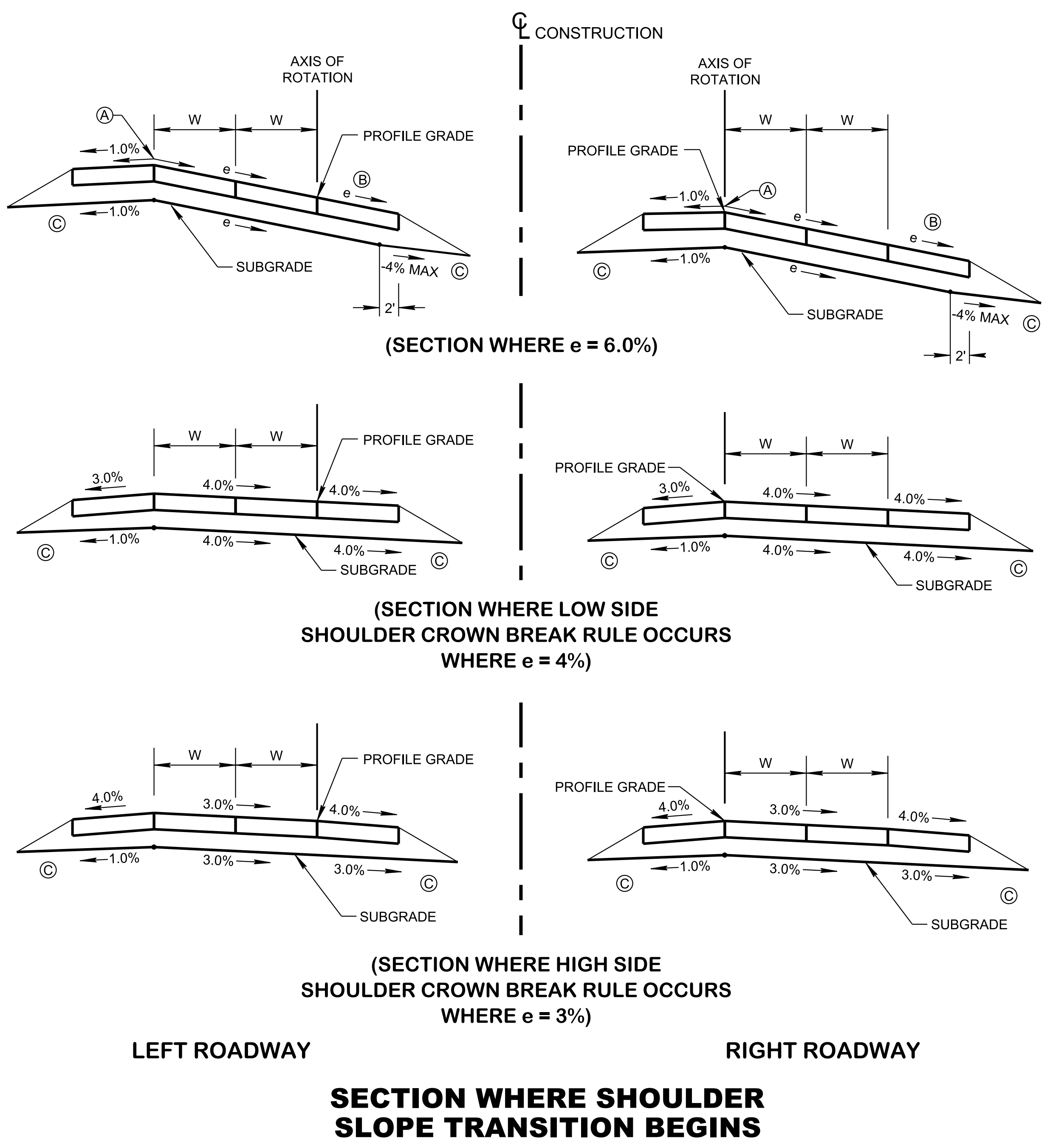
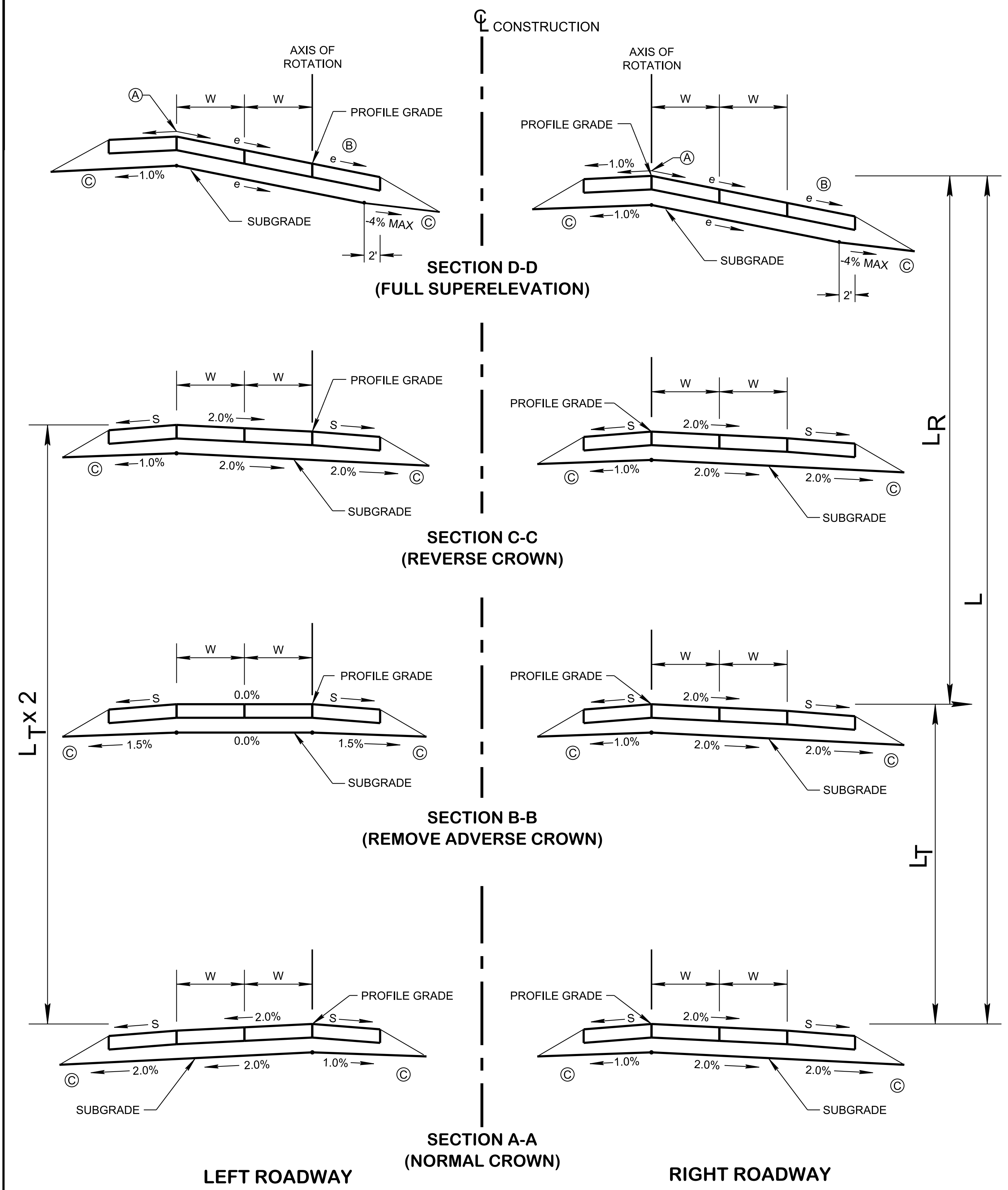


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DESIGN NOTES

(A) HIGH SIDE SHOULDER: MAINTAIN NORMAL SHOULDER CROSS SLOPE (S), UNTIL THE CROSS SLOPE BREAK WITH THE ADJACENT PAVEMENT REACHES A MAXIMUM ALGEBRAIC DIFFERENCE IN GRADES OF 7%. THEN THE SHOULDER SLOPE SHALL BEGIN TO ROTATE TO MAINTAIN THE ALGEBRAIC DIFFERENCE IN GRADES OF 7% BETWEEN THE SHOULDER AND ROADWAY SLOPE.

(B) LOW SIDE SHOULDER: MAINTAIN NORMAL SHOULDER CROSS SLOPE (S) UNTIL THE ADJACENT PAVEMENT SLOPE EQUALS (S), THEN THE SLOPE OF THE SHOULDER ROTATES AT THE SAME CROSS SLOPE AS THE ADJACENT PAVEMENT.

(C) SUBGRADE SLOPE:
 FOR SHOULDERS IN TANGENT :
 THE OUTSIDE SUBGRADE MATCHES THE PAVEMENT SLOPE AND NOT THE SHOULDER SLOPE.
 THE INSIDE SUBGRADE IS -1% FOR LEFT AND RIGHT ROADWAYS.

FOR SHOULDERS IN FULL SUPERELEVATION SECTION:
LOW SIDE MATCHES THE PAVEMENT SLOPE AND NOT THE SHOULDER SLOPE UNTIL SUPERELEVATION REACHES 4%.
 WHEN THE SUPERELEVATION IS GREATER THAN 4% THE SUBGRADE WILL HAVE A BREAK IN SUBGRADE 2' BACK UNDER THE SHOULDER AND THE GRADE RUNNING OUT WILL MAINTAIN A 4% SLOPE. SEE SECTION D-D FOR DETAILS.
HIGH SIDE SHOULDER AND ADJACENT PAVEMENT SUBGRADE CHANGE MUST NOT EXCEED AN ALGEBRAIC DIFFERENCE IN GRADES OF 7%.

WHEN SUBGRADE TRANSITIONS FROM LOW SIDE TO HIGH SIDE ON THE LEFT ROADWAY FOR SUPERELEVATION:
 THE SHOULDER SUBGRADE ON THE OUTSIDE MUST ROTATE UP FROM 2% TO 1% AND THE SHOULDER SUBGRADE ON THE INSIDE MUST ROTATE DOWN FROM 1% TO 2%. THIS SUBGRADE TRANSITION FOR THE INSIDE AND OUTSIDE SHOULDER OCCURS DURING THE TRANSITION FROM NORMAL CROWN TO REVERSE CROWN. SEE SECTIONS A-A, B-B AND C-C FOR DETAILS.

NOT TO SCALE

STATE OF TENNESSEE
 STANDARD DRAWING
 DEPARTMENT OF TRANSPORTATION

**SUPERELEVATION
 TRANSITION
 SECTIONS FOR
 DIVIDED
 ROADWAYS**

01-01-2019 RD11-SE-3A