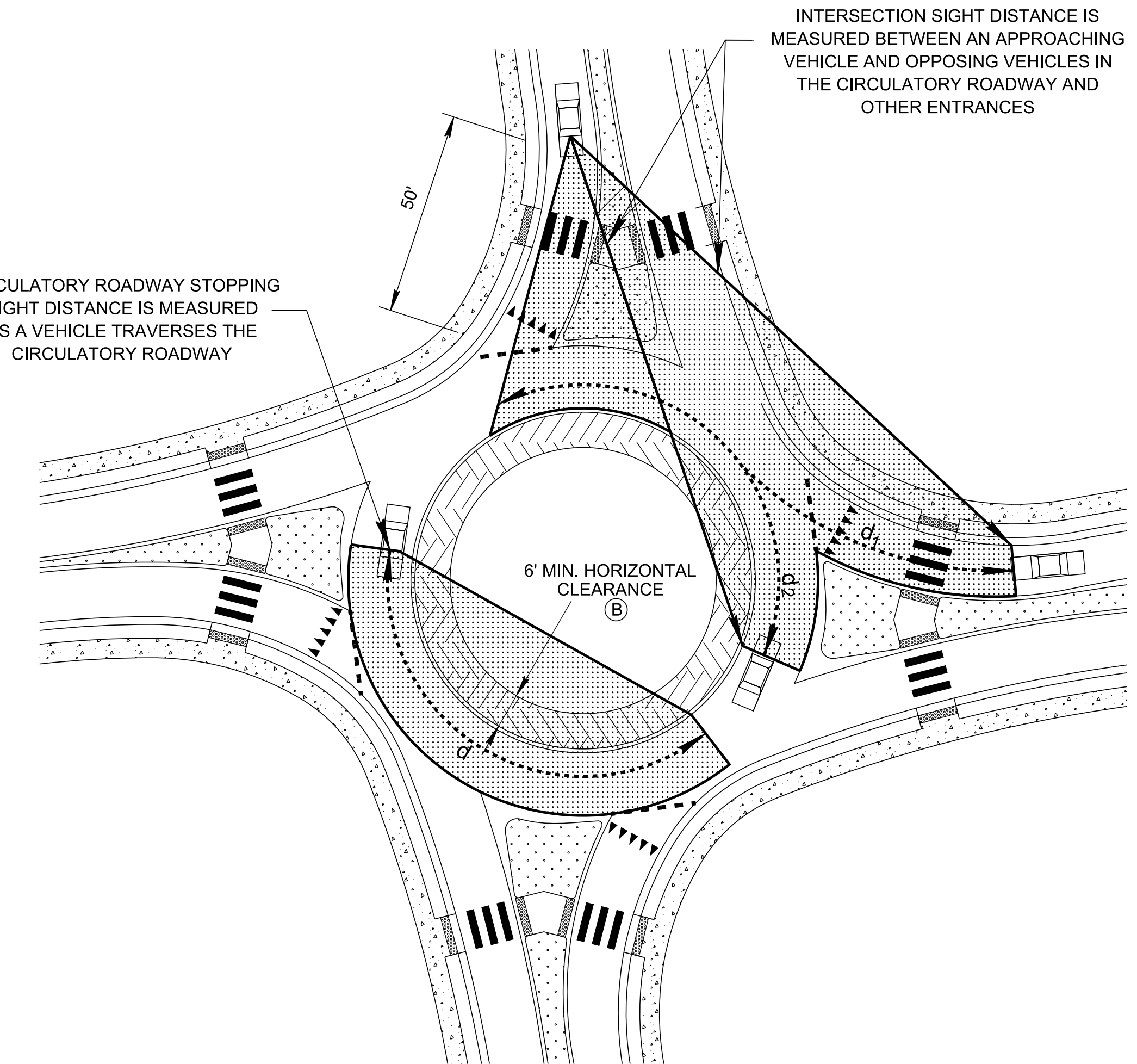


APPROACH STOPPING SIGHT DISTANCE IS MEASURED FROM APPROACHING VEHICLE TO YIELD LINE OR CROSSWALK (IF PED FACILITY EXIST)

STOPPING SIGHT DISTANCE TO CROSSWALK AT EXIT IS MEASURED FROM ENTERING VEHICLE TO CROSSWALK

STOPPING SIGHT DISTANCE ON APPROACH

NOTE: SIGHT DISTANCES SHOWN FOR SINGLE APPROACH LEG, SIMILAR SIGHT DISTANCE FOR THE OTHER LEGS.



CIRCULATORY ROADWAY STOPPING SIGHT DISTANCE IS MEASURED AS A VEHICLE TRAVERSES THE CIRCULATORY ROADWAY

6' MIN. HORIZONTAL CLEARANCE (B)

INTERSECTION SIGHT DISTANCE IS MEASURED BETWEEN AN APPROACHING VEHICLE AND OPPOSING VEHICLES IN THE CIRCULATORY ROADWAY AND OTHER ENTRANCES

STOPPING SIGHT DISTANCE ON CIRCULATORY ROADWAY

LEGEND

- AREAS FREE OF SIGHT OBSTRUCTIONS
- LINE OF SIGHT

DEFINITIONS

- d = STOPPING SIGHT DISTANCE (SSD)
- d_1 = ENTERING STREAM SSD
- d_2 = CIRCULATING STREAM SSD

GENERAL NOTES

- (A) FASTEST PATH CHECKS SHOULD BE COMPLETED PRIOR TO INTERSECTION SIGHT DISTANCE BEING CHECKED. STOPPING SIGHT DISTANCE AND INTERSECTION SIGHT DISTANCE SHOULD BE CHECKED FOR ALL APPROACHES. REFER TO "ROUNDBABOUTS; AN INFORMATIONAL GUIDE," 2ND EDITION, FHWA, 2010, SECTION 6.7 FOR SIGHT DISTANCE.
- (B) PROVIDE 6' MINIMUM UNOBSTRUCTED HORIZONTAL CLEARANCE FROM THE VERTICAL CURB TO THE CENTRAL ISLAND LANDSCAPING TO ALLOW FOR CIRCULATORY ROADWAY SIGHT DISTANCE, ACTUAL DISTANCE MAY BE GREATER AND SHOULD BE DETERMINED AFTER SIGHT DISTANCE CHECKS ARE COMPLETE, BUT SHALL NOT BE LESS THAN 6'.
- (C) SEE STANDARD DRAWINGS:
 RD18-RTS-1 FOR DESIGN STANDARDS FOR SINGLE-LANE ROUNDBABOUTS
 RD18-RTS-2 FOR DESIGN STANDARDS FOR MULTILANE ROUNDBABOUTS
 RD18-RTS-3 FOR STANDARDS CONSTRUCTION DETAILS FOR ROUNDBABOUTS
 T-M-17 FOR PAVEMENT MARKING DETAILS FOR ROUNDBABOUTS
 T-S-25 FOR SIGNING DETAILS FOR ROUNDBABOUTS