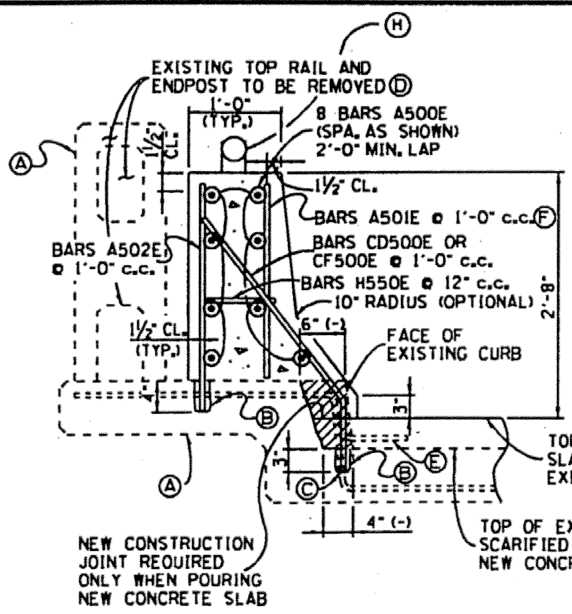


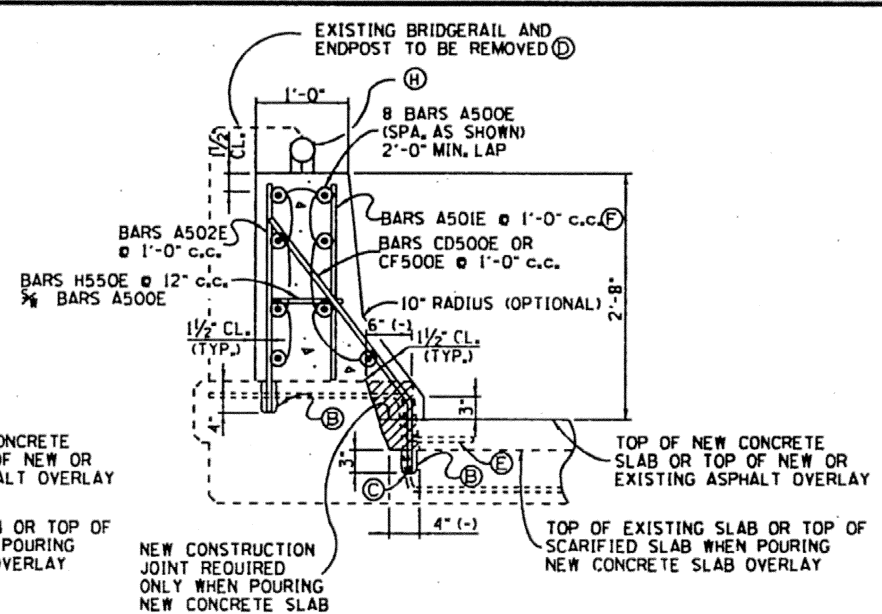
PROJECT NO.	YEAR	SHEET NO.
	1988	

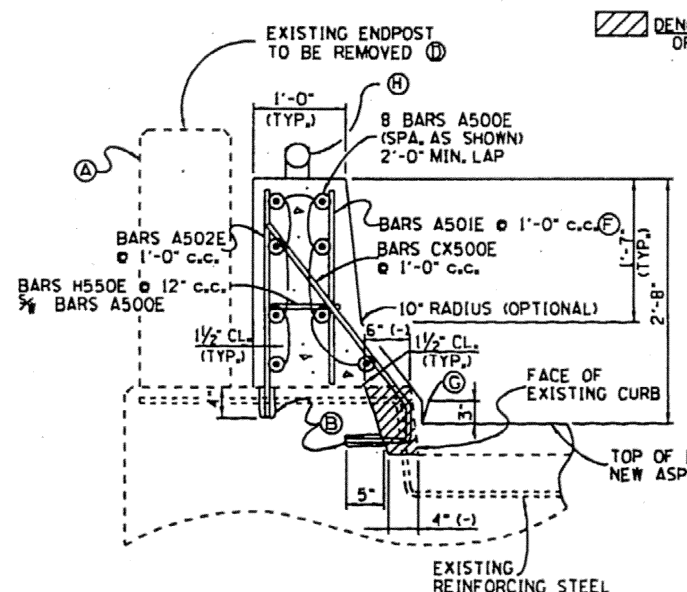
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	8-30-88	RAM	REVISED H550E BARS LOCATION & NOTE
2	9-24-88	RAM	REVISED NOTES & ITEM NUMBER
3	11-19-90	RAM	REVISED TEXTURE COATING DETAIL, ESTIMATED QUANTITIES & NOTES
4	11-16-92	RAM	REVISED DRAWING NOS. & NOTES
5	12-2-94	TO	REVISED NOTE 1, SECTION 8-2 AND REVISED DRAWING NUMBER
6	3-1-95	TO	REVISED GENERAL NOTES, REMOVED SIGNATURE AND ADDED DETAIL OF PARAPET DELINEATOR
7	1-4-96	TO	REVISED BY DRAWING NO. TO SBR DRAWING NO. AND REVISED STD-2-1 TO STD-1-1



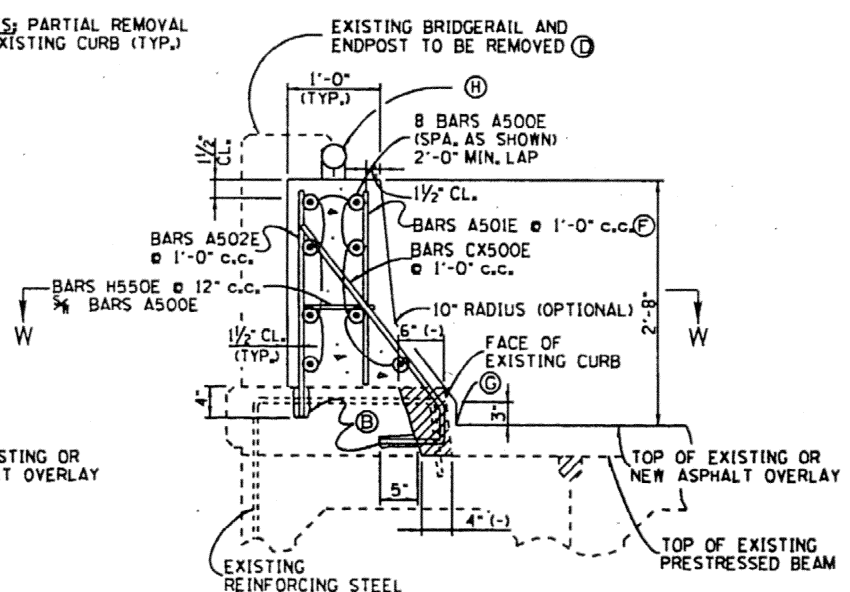
**DETAIL 'A'**  
(SHOWING EXISTING SAFETY CURB)



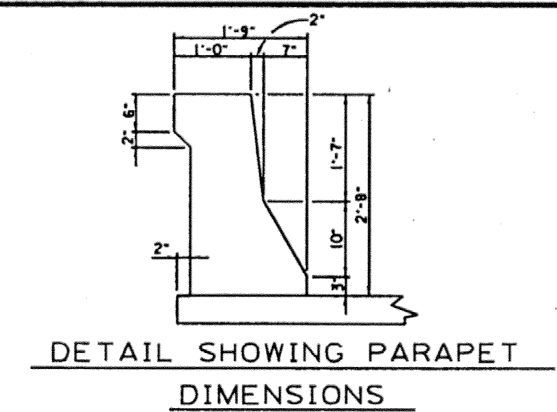
**DETAIL 'C'**  
(SHOWING EXISTING BUFFER CURB)



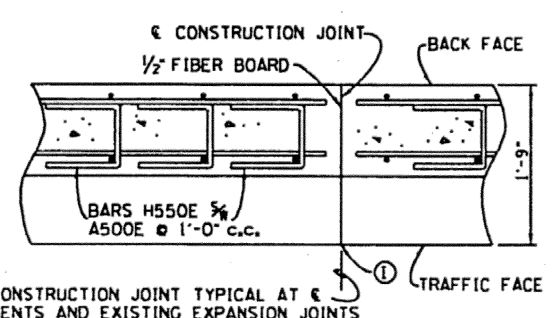
**DETAIL 'B'**  
(SHOWING CONCRETE PARAPET DETAILS NEAR END OF BRIDGE IN ENDWALL AREA)



**DETAIL 'D'**  
(SHOWING CONCRETE PARAPET DETAILS FOR BRIDGES WITH SIDE BY SIDE PRESTRESSED BOX BEAMS OR CHANNELS)

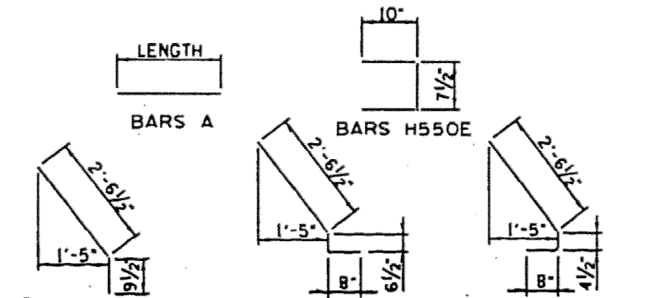


DETAIL SHOWING PARAPET DIMENSIONS



CONSTRUCTION JOINT TYPICAL AT BENTS AND EXISTING EXPANSION JOINTS

**SECTION W - W**



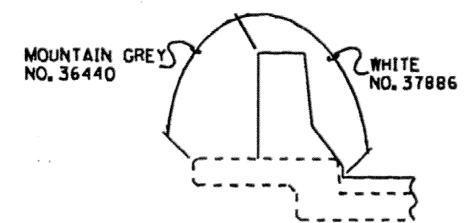
**BAR CD** TOTAL LENGTH = 3'-4"

**BAR CF** TOTAL LENGTH = 3'-9"

**BAR CX** TOTAL LENGTH = 3'-7"

**BAR CD** ARE TO BE USED WHEN PLACING NEW CONCRETE PARAPET ACROSS BRIDGE WITH EXISTING OR NEW ASPHALT OVERLAY.

**BAR CF** MAY BE SUBSTITUTED FOR BARS CD ONLY WHEN PLACING NEW CONCRETE PARAPET ACROSS BRIDGE WITH NEW 1/2" CONCRETE SLAB OVERLAY.



**TEXTURE COATING DETAIL**

(SHOWING LIMITS OF TEXTURE COATING FEDERAL SPECIFICATION NO. 5)

(COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NO. 620-10.01 AND INCLUDES 10'-2" ENDPPOST AS SHOWN ON DRAWING NO. SBR-2-125.)

**ESTIMATED QUANTITIES - PER FOOT**

ITEM	CLASS 'A' CONCRETE C.Y.	EPOXY COATED REINFORCING STEEL LB.	TEXTURED COATED FINISH (NEW) S.Y.
DETAILS 'A' & 'B'	0.12	19.6	1.05
DETAILS 'C' & 'D'	0.12	19.6	0.75

NOTE: ESTIMATED QUANTITIES PER FOOT ARE FOR DETAILS 'A' THRU 'D' FROM BEGINNING TO END OF EXISTING BRIDGE. THE ESTIMATED QUANTITIES FOR THE 10'-2" ENDPPOST AT BRIDGE ENDS ARE SHOWN ON DNG. NO. SBR-2-125, AND SHALL BE INCLUDED IN ITEM NO. 620-10.01, CONCRETE PARAPET (BRIDGERAIL REPLACEMENT), LIN. FT.

**GENERAL NOTES**

**SPECIFICATIONS:** STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION, (CURRENT EDITION)

**DESIGN SPECIFICATIONS:** AASHTO (CURRENT EDITION) WITH ADDENDA.

**CONCRETE:** SHALL BE CLASS 'A' F'C • 3,000 P.S.I. SEE STANDARD SPECIFICATIONS - CONCRETE STRUCTURES.

**REINFORCING STEEL:** SEE STANDARD SPECIFICATIONS.

**FINISHING CONCRETE SURFACES:** A CLASS I FINISH FOLLOWED BY AN APPLIED TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH. SEE STANDARD SPECIFICATIONS. THE COLOR OF THE FINISH SHALL BE AS SHOWN IN TEXTURE COATING DETAIL THIS SHEET, FEDERAL COLOR STANDARD NO. 595A AND A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAVING AND HAULING OPERATIONS AT THE BRIDGE SITE.

**NOTE:** WHEN REMOVING PORTIONS OF THE EXISTING CONCRETE CURBING CARE SHALL BE TAKEN SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXISTING REINFORCING STEEL IN THESE AREAS ARE TO REMAIN IN PLACE AND SHALL BE COMPLETELY CLEANED TO THE FULL SATISFACTION OF THE ENGINEER BEFORE REPAIRING THE NEW CONCRETE PARAPET.

**NOTE:** PROFILE OF CONCRETE PARAPET AND ENDPPOST SHALL CONFORM TO ROADWAY GRADE.

**NOTE:** DETAILS OF GUARDRAIL SHALL CONFORM TO STANDARD DRAWING S-GR-SERIES, EXCEPT FOR DETAILS SHOWN ON THIS SHEET.

**SPECIAL NOTE TO CONTRACTOR WHEN POURING NEW SLAB:** THE CONTRACTOR SHALL POUR THE NEW CONCRETE SLAB TO THE CURB REMOVAL LINE BEFORE POURING NEW PARAPET. BARS CF500E SHALL BE LEFT PROJECTING OUT OF THE NEW SLAB.

**NOTE:** TOPS OF EXISTING CURBS AND EXISTING EXPOSED REINFORCING STEEL SHALL BE BLAST CLEANED BEFORE POURING THE NEW CONCRETE PARAPETS. COSTS OF BLAST CLEANING SHALL BE INCLUDED IN ITEM NO. 620-10.01.

**NOTE:** CONTRACTOR TO POUR NEW CONCRETE JERSEY SHAPE PARAPETS WITH CONSTRUCTION JOINTS AT 6' OF ALL BENTS AND EXISTING EXPANSION JOINTS IN THE EXISTING BRIDGE. COST OF ALL MATERIALS NECESSARY FOR PLACING THE CONSTRUCTION JOINTS AT DESIGNATED LOCATIONS SHALL BE INCLUDED IN ITEM NO. 620-10.01.

**NOTE:** BRIDGES WITH CURB WIDTHS LESS THAN 3'-3" SHALL HAVE EXISTING BRIDGERAIL COMPLETELY REMOVED. BRIDGES WITH CURB WIDTHS 3'-3" OR WIDER SHALL HAVE ONLY THE TOP RAIL AND RAIL POST REMOVED. REINFORCING BARS IN THE POST SHALL BE CUT OFF FLUSH WITH THE TOP OF THE EXISTING CURBS. THE AREA OF POST REMOVAL SHALL BE REPAIRED TO TOP OF THE BOTTOM RAIL. SEE DETAIL 'X' ABOVE. COST OF REMOVING OLD RAILS AND REPAIRING SECTIONS OF BOTTOM RAIL SHALL BE PAID UNDER ITEM NO. 620-10.01.

**NOTE:** COST OF EXISTING CONCRETE CURB REMOVAL, NEW CLASS 'A' CONCRETE, NEW EPOXY COATED REINFORCING STEEL, FORMING, EXISTING BRIDGERAIL REMOVAL, BLAST CLEANING, DRILLING, CONCRETE GROUTS, TEXTURED COATING, EXCAVATION, BACKFILLING, LABOR AND ALL MISCELLANEOUS MATERIALS NECESSARY TO BUILD THE NEW CONCRETE PARAPET WITH NEW 10'-2" CONCRETE ENDPPOST AT BRIDGE ENDS SHALL BE INCLUDED IN ITEM NO. 620-10.01.

MINOR REVISION - FHWA APPROVAL NOT REQUIRED

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPPOST  
1988

DESIGNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
DRAWN BY: SIM. FRANKENFIELD DATE: 9-92  
SUPERVISED BY: GENTRY & McINTURFF DATE: 9-92  
CHECKED BY: McINTURFF DATE: 9-92