

REV. 9-18-89: ADDED NOTES FOR CLAMPS AND SHAFT LENGTHS. CHANGED MAX. SAG TO 5 PERCENT ADDED DETAILS FOR CONNECTORS AND CABLE CLAMPS.

REV. 7-29-96: REDREW SHEET ON CADD AND MADE MINOR CHANGES.

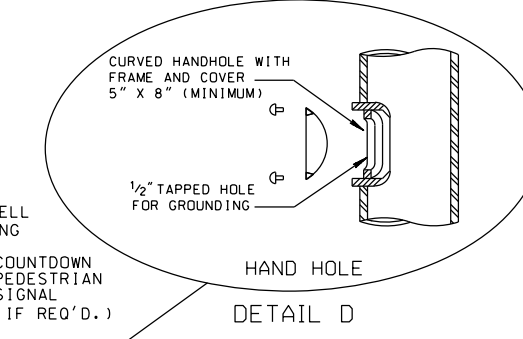
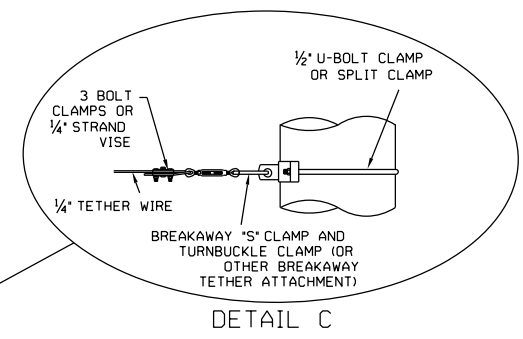
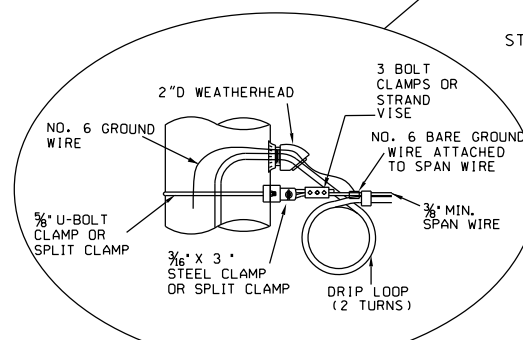
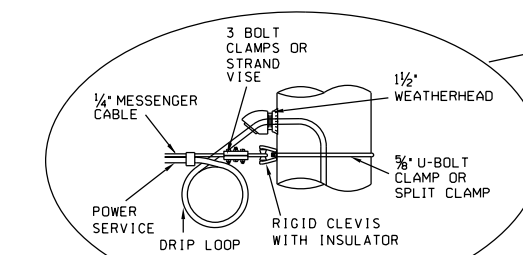
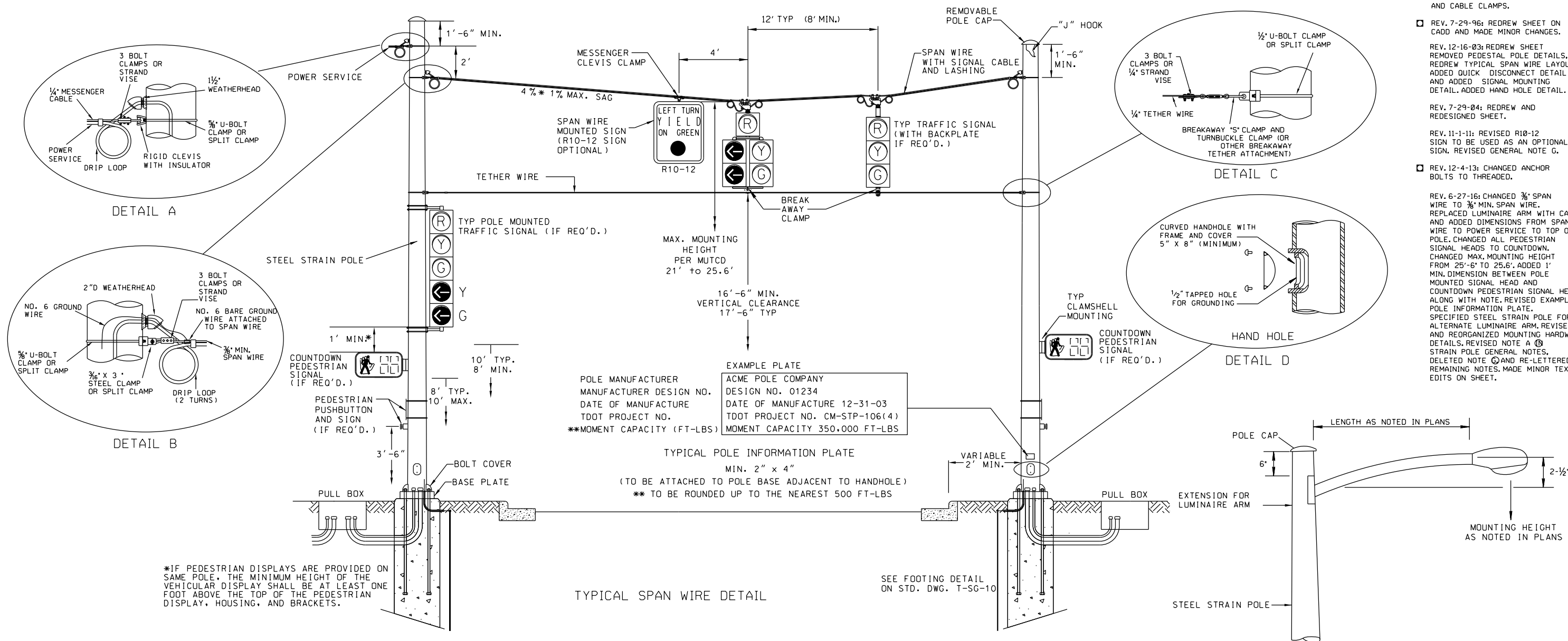
REV. 12-16-03: REDREW SHEET REMOVED PEDESTAL POLE DETAILS, REDREW TYPICAL SPAN WIRE LAYOUT, ADDED QUICK DISCONNECT DETAIL AND ADDED SIGNAL MOUNTING DETAIL. ADDED HAND HOLE DETAIL.

REV. 7-29-04: REDREW AND REDESIGNED SHEET.

REV. 11-11-11: REVISED R10-12 SIGN TO BE USED AS AN OPTIONAL SIGN. REVISED GENERAL NOTE G.

REV. 12-4-13: CHANGED ANCHOR BOLTS TO THREADED.

REV. 6-27-16: CHANGED $\frac{3}{8}$ " SPAN WIRE TO $\frac{1}{2}$ " MIN. SPAN WIRE. REPLACED LUMINAIRE ARM WITH CAP AND ADDED DIMENSIONS FROM SPAN WIRE TO POWER SERVICE TO TOP OF POLE. CHANGED ALL PEDESTRIAN SIGNAL HEADS TO COUNTDOWN. CHANGED MAX. MOUNTING HEIGHT FROM 25'-6" TO 25.6'. ADDED 1" MIN. DIMENSION BETWEEN POLE MOUNTED SIGNAL HEAD AND COUNTDOWN PEDESTRIAN SIGNAL HEAD ALONG WITH NOTE, REVISED EXAMPLE POLE INFORMATION PLATE. SPECIFIED STEEL STRAIN POLE FOR ALTERNATE LUMINAIRE ARM. REVISED AND REORGANIZED MOUNTING HARDWARE DETAILS. REVISED NOTE A TO STRAIN POLE GENERAL NOTES. DELETED NOTE C AND RE-LETTERED REMAINING NOTES. MADE MINOR TEXT EDITS ON SHEET.

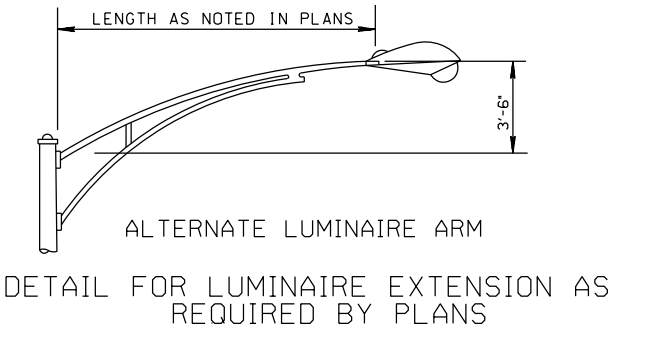
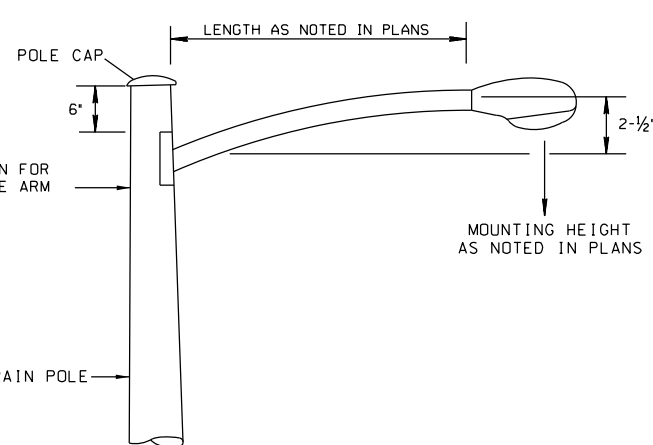
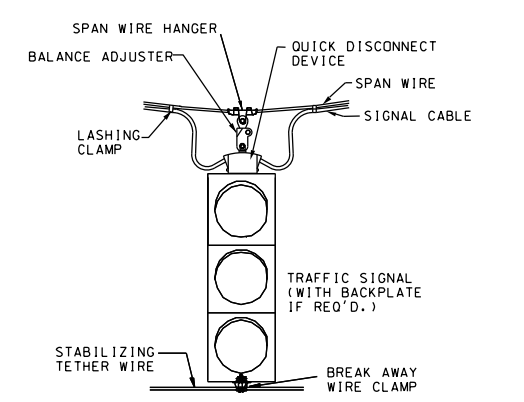
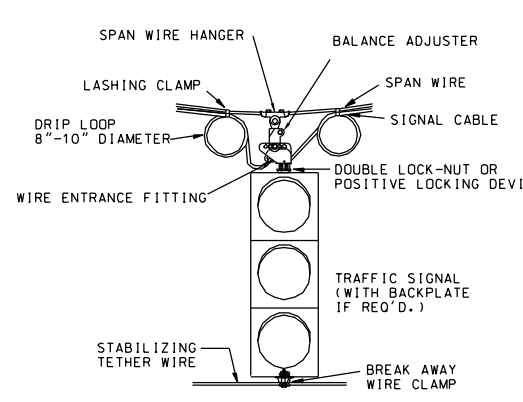


POLE MANUFACTURER
 MANUFACTURER DESIGN NO. ACME POLE COMPANY
 DATE OF MANUFACTURE 12-31-03
 TDOT PROJECT NO. CM-STP-106(4)
 **MOMENT CAPACITY (FT-LBS) MOMENT CAPACITY 350,000 FT-LBS

TYPICAL POLE INFORMATION PLATE
 MIN. 2" x 4"
 (TO BE ATTACHED TO POLE BASE ADJACENT TO HANDHOLE)
 ** TO BE ROUNDED UP TO THE NEAREST 500 FT-LBS

*IF PEDESTRIAN DISPLAYS ARE PROVIDED ON SAME POLE, THE MINIMUM HEIGHT OF THE VEHICULAR DISPLAY SHALL BE AT LEAST ONE FOOT ABOVE THE TOP OF THE PEDESTRIAN DISPLAY, HOUSING, AND BRACKETS.

- STRAIN POLE GENERAL NOTES**
- (A) LOCATIONS OF SIGNAL POLES SHOWN ON PLANS ARE APPROXIMATE AND CAN BE ADJUSTED UP TO 2' TO AVOID UTILITIES. ADJUSTMENTS GREATER THAN 2' MUST BE REVIEWED AND APPROVED BY ENGINEER.
 - (B) ALL STRAIN POLES AT AN INTERSECTION SHALL BE SAME DIAMETER AND BOLT CIRCLE.
 - (C) TYPICAL AERIAL POWER SERVICE ENTRANCE IS THROUGH WEATHER HEAD AND DOWN POLE INTERNALLY. IF CABINET IS POLE MOUNTED, AS AN ALTERNATE POWER SERVICE CABLE MAY BE RUN DOWN OUTSIDE OF STEEL STRAIN POLE IN 1" RGS RISER. UNDERGROUND POWER SERVICE SHALL BE RUN THROUGH SEPARATE 1" RGS CONDUIT THROUGH POLE FOUNDATION (SEE STD. DWG. T-SG-5).
 - (D) POWER SERVICE CABLE SHALL BE RUN ON SEPARATE MESSENGER CABLE (2' ABOVE SPAN WIRE WITH SIGNAL OR DETECTOR CABLES).
 - (E) ADDITIONAL WEATHER HEADS MAY BE INSTALLED FOR EASE OF CABLE ENTRANCE IF NECESSARY (TO BE FIELD DRILLED AND TAPPED).
 - (F) ENTRANCES FOR POLE MOUNTED SIGNAL HEADS SHALL BE FIELD DRILLED TO ENSURE PROPER PLACEMENT.
 - (G) RED INDICATIONS TO BE APPROXIMATELY SAME HEIGHT. HANGER CONNECTOR EXTENDER OR TETHER EXTENDER MAY BE REQUIRED.
 - (H) PEDESTRIAN SIGNAL HEADS TO BE CLAMSHELL MOUNTED UNLESS OTHERWISE SHOWN ON PLANS.
 - (I) SEE STD. DWG. T-SG-4 FOR ADDITIONAL DETAILS.



MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.